



**VILLAGE OF BELCARRA
REGULAR COUNCIL AGENDA
Village Hall
February 21, 2023
7:00 PM**



*This meeting is lived streamed and recorded by the Village of Belcarra
To view the meeting click: [Village of Belcarra - YouTube](#)*

We wish to acknowledge that this meeting is taking place on the unceded territory of the Coast Salish Peoples. Tum-Tumay-Whueton, or Belcarra, is home to an ancestral village of the Tsleil-Waututh Nation. We are thankful to conduct our work within their territory.

COUNCIL

Mayor Jamie Ross
Councillor Carolina Clark
Councillor Joe Elworthy
Councillor Janet Ruzycki
Councillor Liisa Wilder

1. CALL TO ORDER

Mayor Ross will call the meeting to order.

2. APPROVAL OF THE AGENDA

2.1 Regular Council Meeting, February 21, 2023

Recommendation:

That the agenda for the Regular Council Meeting, February 21, 2023 be approved as circulated.

3. ADOPTION OF MINUTES

3.1 Regular Council Meeting, February 6, 2023

Recommendation:

That the minutes from the Regular Council Meeting held February 6, 2023 be adopted.

4. DELEGATIONS AND PRESENTATIONS

5. REPORTS

- 5.1** Ken Bjorgaard, Financial Consultant, report dated February 21, 2023 regarding the 5-Year (2023 – 2027 Financial Plan Bylaw.

Recommendation:

That Village of Belcarra 5-Year (2023 – 2027) Financial Plan Bylaw No. 606, 2023 be read a first and second time.

- 5.2** Ken Bjorgaard, Financial Consultant, report dated February 21, 2023, regarding the Village of Belcarra Fees and Charges Bylaw.

Recommendation:

That Village of Belcarra Fees and Charges Bylaw No. 517, 2018 Amendment Bylaw No. 607, 2023 be read a first and second time.

- 5.3** Ken Bjorgaard, Financial Consultant, report dated February 21, 2023, regarding the Water Parcel Tax Amendment Bylaw.

Recommendation:

That Village of Belcarra Water Supply and Distribution Local Area Service No. 1 Parcel Tax Bylaw No. 452, 2012 Amendment Bylaw No. 608, 2023 be read a first and second time.

- 5.4** Paula Richardson, Chief Administration Officer, report dated February 21, 2023 regarding UBCM Group Benefits for Elected Officials in B.C., Village of Belcarra Council Members

Recommendation:

That staff be directed to enroll those members of Council who wish to participate, in the Union of British Columbia Municipalities Group Benefits Plan for Elected Officials.

- 5.5** Stewart Novak, Public Works and Emergency Preparedness Coordinator, report dated February 21, 2023 regarding Marine Avenue Road Rehabilitation, Engineering Quote.

Recommendation:

That Council approve the quote submitted by ISL Engineering in the sum of \$19,515.00 plus applicable taxes for work relating to the 3700 block Marine Avenue Road rehabilitation.

6. REPORTS FROM MAYOR AND PROJECT LEADS

6.1 Mayor's Report

- Mayor Ross highlighted the Provincial Government announcement related to funds for municipalities below:
 - Premier David Eby announces \$1 billion in grants for municipalities:
 - All of B.C.'s 188 municipalities and regional districts will be eligible to tap into the \$1-billion Growing Communities Fund which can be used to build new infrastructure and amenities such as recreation facilities, transit services, parks and water treatment plants (see Item 9.4)
- Mayor Ross attended a Mayors' Council Public Affairs & Governance Committee meeting on February 15, 2023

7. REPORT FROM CHIEF ADMINISTRATIVE OFFICER

- 7.1** Paula Richardson, Chief Administration Officer, report dated February 21, 2023 regarding NAV CANADA – Changes to Flight Paths in Greater Vancouver Region and Southern Vancouver Island

Recommendation:

That staff be directed to write a letter to NAV CANADA expressing concern with proposed route changes to flight paths over the Tri-Cities area for aircraft accessing Vancouver International Airport.

8. BYLAWS

8.1 Village of Belcarra Council Indemnity Bylaw No. 604, 2023

A bylaw to provide for the payment of an indemnity to Village of Belcarra Mayor and Council

Recommendation:

That the Village of Belcarra Council Indemnity Bylaw No. 604, 2023 be adopted.

9. CORRESPONDENCE/PROCLAMATIONS

Recommendation:

That correspondence items 9.1 to 9.5 be received.

ACTION ITEMS

- 9.1** Heather Skipworth, Port Moody Secondary School Parent AfterGrad Committee, email dated February 4, 2023 requesting a donation for a 2023 AfterGrad event.

Recommendation:

That the Port Moody Secondary School Class of 2023 AfterGrad event be supported with financial assistance in the amount of \$150.00

INFORMATION ITEMS

- 9.2** Lynsay Pacey, Program Officer, Local Government Services, Union of BC Municipalities (UBCM), letter advising that the Village of Belcarra is the recipient of a \$20,000 grant to assist with the incorporation of 2021 Active Transportation Planning during the Official Community Plan update.
- 9.3** David Barwin, Belcarra Resident, letter dated February 7, 2023, regarding the need for a emergency alert system serving the boat access communities of Indian Arm and adjacent municipalities, including Belcarra.
- 9.4** Anne King, Minister, Ministry of Municipal Affairs, letter dated February 10, 2023, providing information on grant funding to BC municipalities and regional districts to support a growing need for infrastructure and amenities.
- 9.5** Walter Mechler, letter dated February 13, 2023, requesting that charges for the cost of operation and maintenance of the recycle/refuse utility fee for non-users whose primary residence is elsewhere in Metro Vancouver be waived and a memorandum outlining the impact of the Speculation and Vacancy Tax (SVT).

10. NEW BUSINESS

11. PUBLIC QUESTION PERIOD

12. ADJOURNMENT

Recommendation:

That the February 21, 2023 Regular Meeting be adjourned.



**VILLAGE OF BELCARRA
REGULAR COUNCIL MINUTES
February 6, 2023**



This meeting was held in Council Chambers and was livestreamed on the
[Village of Belcarra - YouTube](#) channel

Council in Attendance

Mayor Jamie Ross
Councillor Carolina Clark
Councillor Joe Elworthy (participated via Zoom)
Councillor Janet Ruzycki
Councillor Liisa Wilder

Staff in Attendance

Paula Richardson, Chief Administrative Officer
Stewart Novak, Public Works & Emergency Preparedness Coordinator
Connie Esposito, Accounting Clerk
Amanda Seibert, Corporate Officer/Recording Secretary

Others in Attendance

Louis Orioux, B.A. Blackwell and Associates Ltd.
Ken Bjorgaard, Financial Consultant

We wish to acknowledge that this meeting took place on the unceded territory of the Coast Salish peoples. Tum-Tumay-Whueton, or Belcarra, is home to an ancestral village of the Tsleil-Waututh Nation. We are thankful to conduct our work within their territory.

Councillor Elworthy was not in attendance at the start of the meeting.

1. CALL TO ORDER

Mayor Ross called the meeting to order at 6:59 pm.

2. APPROVAL OF THE AGENDA

2.1 Regular Council Meeting, Date

Moved by: Councillor Clark
Seconded by: Councillor Wilder

That the agenda for the Regular Council Meeting of February 6, 2023 be approved as circulated.

CARRIED

3. ADOPTION OF MINUTES

3.1 Regular Council Meeting, January 23, 2023

Moved by: Councillor Ruzyski
Seconded by: Councillor Clark

That the minutes from the Regular Council Meeting held on January 23, 2023 be adopted as circulated.

CARRIED

4. DELEGATIONS AND PRESENTATIONS

4.1 Louis Orieux, B.A. Blackwell and Associates Ltd., presentation on the Village of Belcarra's Interface Wildfire Development Permit Area (DPA) Policy Development

Mr. Orieux provided a presentation on an Interface Wildfire Development Permit Area (DPA) Policy. He outlined the purpose of the development of a DPA policy and the scope of the project. He provided maps indicating wildfire behaviour threats and advised on fire behaviour within communities.

Councillor Elworthy joined the meeting at 7:08 pm during the presentation.

Mr. Orieux advised on the FireSmart Home Ignition Zone and the key policies designed to limit fire spread within a community. He responded to questions from members of Council and residents attending the meeting.

Moved by: Councillor Clark
Seconded by: Councillor Wilder

That the presentation by B.A. Blackwell and Associates Ltd. on the Village of Belcarra's Interface Wildfire Development Permit Area (DPA) Policy Development be received for information.

CARRIED

5. REPORTS

5.1 Ken Bjorgaard, Financial Consultant, report dated February 6, 2023, regarding the Village of Belcarra 2023 – 2027 Financial Plan.

K. Bjorgaard presented a final report on the Draft 2023 – 2027 Financial Plan which will allow the development of a financial plan bylaw as a next step. He noted the difference in the recommendation provided to Council and advised that previously provided Scenarios A and B were combined to help eliminate the deficit with WARD

Discussion ensued with regard to the purchase of vehicles and the replacement strategy.

Mayor Ross invited residents to bring forward questions or comments.

Jim Chisolm queried with regard to funding for the fire halls.

K. Bjorgaard stated that Metro Vancouver holds the reserve funds for the fire halls and fire apparatus and that a portion of those reserves are designated for fire hall replacement.

The Mayor advised that further information on the fire halls will be provided at a future meeting.

Jim Chisolm queried as to the total line of credit for the Village.

K. Bjorgaard advised that a formula for lines of credit for municipalities is set by regulation under the *Community Charter* and that the information can be provided.

Klaus Bever requested rationale on the additional rates for WARD.

K. Bjorgaard advised on the additional rates.

Deborah Struk queried with regard to deferral of taxes.

Staff advised that tax deferrals are only permitted on property taxes and that water and utilities fees cannot be deferred as these fees fund operations.

Moved by: Councillor Elworthy
Seconded by: Councillor Clark

That the Village of Belcarra's 2023 – 2027 Financial Plan bylaw and water and waste & recycle depot (WARD) user rates bylaws be prepared on the basis of Scenario A as follows:

- 3% increase in property taxes in 2023;
- 3% increase in property taxes in each year from 2024 to 2027;
- 10% increase in water user rates in 2023;
- 10% increase in water user rates in each year from 2024 to 2027;
- 12% increase in WARD user rates in 2023;
- 12% increase in WARD user rates in each year from 2024 to 2027.

CARRIED

5.2 Ken Bjorgaard, Financial Consultant, report dated February 6, 2023 regarding Grant-in-Aid Requests including background information.

K. Bjorgaard reviewed the report and provided background on what has been budgeted for grants in previous years.

Moved by: Councillor Wilder
Seconded by: Councillor Ruzycki

That staff be directed to provide grants to organizations requesting financial assistance as outlined in the motions below:

- (a) That the Belcarra Barnacle be supported with financial assistance in the amount of \$1000.00.
- (b) That the Community Youth Justice Program be supported with financial assistance in the amount of \$353.00.
- (c) That the 2023 Heritage Woods Secondary School Dry Grad be supported with financial assistance in the amount of \$150.00

Council requested that the motions on the floor be dealt with separately.

Moved: Councillor Elworthy
Seconded: Councillor Clark

That sections a), b) and c) of the motion to provide grants to organizations requesting financial assistance be dealt with separately.

CARRIED
Mayor Ross voted in opposition

Discussion ensued with regard to the history of the Barnacle Newspaper, the work carried out by volunteers and the historical involvement of the paper within the community.

Moved by: Councillor Wilder
Seconded by: Councillor Elworthy

- (a) That the Belcarra Barnacle be supported with financial assistance in the amount of \$1000.00.

CARRIED
Councillor Clark, Councillor Ruzycki voted in opposition

Moved: Councillor Ruzycki
Seconded: Councillor Clark

- (b) That the Community Youth Justice Program be supported with financial assistance in the amount of \$353.00.

CARRIED

Moved: Councillor Wilder
Seconded: Councillor Ruzycki

- (c) That the 2023 Heritage Woods Secondary School Dry Grad be supported with financial assistance in the amount of \$150.00

CARRIED

5.3 Stewart Novak, Public Works and Emergency Preparedness Coordinator, report dated February 6, 2023 regarding Disposal of Assets - 2011 Coachmen Catalina 20-Foot Travel Trailer.

The Public Works and Emergency Preparedness Coordinator reviewed the report regarding the disposal of a Coachmen Catalina travel trailers by auction. He advised that an announcement will be made as to the actual date of the auction.

Discussion ensued regarding methods of disposing of the trailer. Staff emphasized that due to a mold issue with the trailer it was preferred that the asset be disposed of through auction.

Mayor Ross invited residents to bring forward questions or comments.

Jim Chisholm asked if the Village will advise regarding the mold issue and whether the new trailer for the Building Inspector is being inspected and maintained.

The Public Works and Emergency Preparedness Coordinator advised that the auction company will be informed.

Mayor Ross indicated that a future report on the maintenance of the trailer for the Building Inspector may be provided.

Moved by: Councillor Wilder
Seconded by: Councillor Clark

That the 2011 Coachmen Catalina 20-foot travel trailer purchased in 2015 by the Village of Belcarra in the amount of \$14,199.00, be disposed of through public auction.

CARRIED

The meeting recessed at 8:59 pm and reconvened at 9:15 pm.

6. REPORTS FROM MAYOR AND PROJECT LEADS

6.1 Mayor's Report

- On February 5, 2023, Mayor Ross attended a Metro Vancouver hosted dinner for the Council of Mayors South East Queensland (COMSEQ). In part, their visit focused on learning about Climate 2050 and examples in the region; National Zero Waste Council (circular economy) - ways that Metro Vancouver is looking to build a resilient region. Their trip included a visit to the United Boulevard Recycling and Waste Centre.

Mayor Ross attended the following meetings on February 1-3, 2023:

- Metro Vancouver Regional Parks Committee
- Metro Vancouver Mayors' Committee
- Metro Vancouver Climate Action Committee
- Metro Vancouver Board Special Meeting (Orientation)

Mayor Ross invited residents to bring forward questions or comments.

Jim Chisholm queried whether Metro Vancouver Parks had provided information to the Village about the property where the caretaker's house was demolished.

Staff will request an update from Metro Vancouver Parks.

7. REPORT FROM CHIEF ADMINISTRATIVE OFFICER

The Chief Administrative Officer read a statement outlining the sequence of events which took place on November 27, 2023 when a tree fell blocking Bedwell Bay Road near the Village Hall. She outlined the response on the day of the event.

Discussion ensued with regard to the process followed when the road was blocked

8. BYLAWS

8.1 Village of Belcarra Council Indemnity Bylaw No. 604, 2023

A bylaw to provide for the payment of an indemnity to Village of Belcarra Mayor and Council

Moved by: Councillor Clark
Seconded by: Councillor Ruzyski

That the Village of Belcarra Council Indemnity Bylaw No. 604, 2023 be read a third time.

CARRIED

8.2 Village of Belcarra West Road Renaming Bylaw No. 603, 2023

A bylaw to change the name of West Road to Marine Avenue

Moved by: Councillor Wilder
Seconded by: Councillor Clark

That Village of Belcarra West Road Renaming Bylaw No. 603, 2023 be adopted.

CARRIED

9. CORRESPONDENCE/PROCLAMATIONS

Moved by: Councillor Wilder
Seconded by: Councillor Clark

That correspondence items 9.1 to 9.2 be received into the record.

ACTION ITEMS

No items.

INFORMATION ITEMS

9.1 Shannon Story, Executive Director, Lower Mainland Local Government Association, email dated January 12, 2023, regarding:

- a) 2023 Conference & AGM - Resolutions Notice – Request for Submissions
- b) 2023 Call for Nominations for Lower Mainland LGA Executive

9.2 Kerri Palmer Isaak, School District 43 Trustee, Community Update Newsletter for Belcarra and Anmore.

10. NEW BUSINESS

The Chief Administrative Officer reviewed correspondence received from the Garden Club requesting rental of the Village Hall. She noted that the Village's Municipal Property Booking Policy provides for the CAO to waive fees to groups listed in the policy. She advised that although the Garden Club is not on the list of groups, requested the support of Council to waive the fees associated with renting the Hall. Council members supported the request to waive fees. It was noted that all groups using the Hall, whether they pay or not, must arrange insurance through MIABC.

11. PUBLIC QUESTION PERIOD

Jim Chisholm asked if the Village is looking at other sources other than the parcel tax in regard to financing a new fire hall.

Mayor Ross advised that a report will be provided at a later date.

12. ADJOURNMENT

Moved by: Councillor Wilder
Seconded by: Councillor Ruzycki

That the February 6, 2023 Regular Meeting be adjourned at 9:44 pm

CARRIED

Certified Correct:

Jamie Ross
Mayor

Paula Richardson
Chief Administrative Officer



COUNCIL REPORT

Date: February 21, 2023
From: Ken Bjorgaard, Financial Consultant
Subject: 5-Year (2023 – 2027) Financial Plan Bylaw

Recommendation:

That Village of Belcarra 5-Year (2023 – 2027) Financial Plan Bylaw No. 606, 2023 be read a first and second time.

Purpose:

The purpose of this report is to present the Village's 2023 – 2027 Financial Plan Bylaw (Bylaw) for first and second readings. This Bylaw reflects the results of Council's budget deliberations and related decisions and has been prepared pursuant to the following February 6, 2023 Council motion:

"That the Village of Belcarra's 2023 – 2027 Financial Plan bylaw and water and waste & recycle depot (WARD) user rates bylaws be prepared on the basis of the following:

- 3% increase in property taxes in 2023;
- 3% increase in property taxes in each year from 2024 to 2027;
- 10% increase in water user rates in 2023;
- 10% increase in water user rates in each year from 2024 to 2027;
- 12% increase in WARD user rates in 2023;
- 12% increase in WARD user rates in each year from 2024 to 2027."

Background:

The Bylaw attached hereto for Council's consideration reflects the above noted Council motion. It should be noted that Schedules A & B which are attached to the Bylaw, and form part thereof, are a requirement of the *Community Charter*. It is recommended that the Bylaw receive first two (2) readings on February 21, 2023. Third reading is scheduled to occur on March 6, 2023 and adoption on March 27, 2023. The *Community Charter* allows financial plans to be amended by bylaw after adoption.

Appendix A: Village of Belcarra 5-Year (2023 – 2027) Financial Plan Bylaw No. 606, 2023



**VILLAGE OF BELCARRA
5-Year (2023 – 2027) Financial Plan
Bylaw No. 606, 2023**



A bylaw to establish the 5-Year Financial Plan for the years 2023 – 2027 inclusive.

WHEREAS pursuant to section 165 of the *Community Charter*, the Council shall, before the 15th of May in each year, before the annual property tax bylaw is adopted, adopt a financial plan;

AND WHEREAS the Municipal Council has caused to be prepared a 5-Year Financial Plan for the period 2023 – 2027 inclusive;

NOW THEREFORE, the Council of the Village of Belcarra enacts as follows:

1. This Bylaw shall be cited for all purposes as the “Village of Belcarra 5-Year (2023 – 2027) Financial Plan Bylaw No. 606, 2023”.
2. Council hereby adopts the 5-Year Financial Plan for the years 2023 – 2027 inclusive, as set out in Schedules A and B, attached to and forming part of this bylaw.
3. If a portion of this bylaw is held invalid by a Court of competent jurisdiction, then the invalid portion must be severed and the remainder of this bylaw is deemed to have been adopted without the severed section, subsection, paragraph, subparagraph, clause or phrase.

READ A FIRST TIME on

READ A SECOND TIME on

READ A THIRD TIME on

ADOPTED by the Council on

Jamie Ross
Mayor

Paula Richardson
Chief Administrative Officer

This is a certified a true copy of
Village of Belcarra 5-Year (2023 – 2027) Financial
Plan Bylaw No. 606, 2023

Chief Administrative Officer

Schedule A - Financial Plan

	2023	2024	2025	2026	2027
REVENUE					
Taxation (including grants in lieu)	(1,006,297)	(1,036,380)	(1,067,363)	(1,099,273)	(1,132,139)
Parcel taxes	(267,617)	(267,617)	(267,617)	(267,617)	(267,617)
Sale of services & regulatory fees	(685,699)	(746,355)	(813,326)	(887,294)	(969,009)
Government transfers	(529,119)	(537,677)	(502,279)	(519,477)	(776,167)
Investment income	(57,693)	(58,425)	(59,171)	(59,930)	(60,704)
Actuarial income	(44,814)	(50,547)	(56,482)	(62,623)	(68,980)
Total Revenue	(2,591,239)	(2,697,001)	(2,766,238)	(2,896,214)	(3,274,616)
EXPENSES					
General government & fiscal services	223,262	228,842	234,474	265,198	241,798
Administration & human resources	267,901	273,757	285,942	291,535	297,237
Information technology	40,636	41,478	42,347	43,243	44,166
Support services (engineering, finance & planning)	158,985	140,854	142,757	144,697	146,671
Building inspection & bylaw enforcement	87,334	89,057	90,813	92,605	94,432
Public works & transportation	255,585	250,905	255,063	268,331	272,825
Major road network (MRN)	168,196	170,403	173,628	176,923	179,683
Fire & emergency services	7,454	7,492	7,530	7,568	7,607
Waste & recycle depot (WARD)	187,873	192,246	198,155	204,252	209,690
Water system	514,922	529,209	555,788	579,713	605,853
Amortization	354,090	371,555	389,889	409,135	429,339
Total Expenses	2,266,238	2,295,798	2,376,386	2,483,200	2,529,301
ANNUAL SURPLUS	(325,001)	(401,203)	(389,852)	(413,014)	(745,315)
RESERVES, DEBT & CAPITAL					
Tangible capital assets	550,100	405,500	200,000	253,000	436,000
Amortization	(354,090)	(371,555)	(389,889)	(409,135)	(429,339)
Repayment of debt (principal & actuarial)	163,816	169,549	175,484	181,625	187,982
Transfers from reserves	(555,146)	(396,196)	(231,946)	(300,748)	(205,446)
Transfers to reserves	520,947	569,038	609,642	656,680	724,494
Transfer from appropriated surplus	(9,400)	-	-	-	-
Transfers from surplus	(17,726)	(1,663)	-	-	-
Transfers to surplus	26,500	26,530	26,561	31,592	31,624
Total Reserves, Debt & Capital	325,001	401,203	389,852	413,014	745,315
FINANCIAL PLAN BALANCE	-	-	-	-	-

Schedule B – Statement of Objectives and Policies

Financial Plan Objectives and Policies for Funding Sources, Distribution of Property Value Taxes, and Permissive Tax Exemptions

A. Funding Sources

Over the Village of Belcarra's 5-year financial plan the funding sources, as defined in Section (165) (7) of the Community Charter, are as follows:

	<u>Funding Sources</u>				
	2023	2024	2025	2026	2027
Operating Revenue Sources					
Property value taxation	39.3%	39.0%	38.6%	38.0%	37.5%
Parcel tax	10.5%	10.1%	9.7%	9.3%	8.9%
Fees	26.9%	28.1%	29.4%	30.8%	32.2%
Other sources*	23.3%	22.8%	22.3%	21.9%	21.4%
Proceeds from borrowing	0.0%	0.0%	0.0%	0.0%	0.0%
Totals	100.0%	100.0%	100.0%	100.0%	100.0%
Capital Revenue Sources					
Other sources - Reserves	91.5%	89.8%	100.0%	95.6%	39.8%
Other sources - Grants	6.8%	10.2%	0.0%	4.4%	60.2%
Other sources - Appropriated Surplus	1.7%	0.0%	0.0%	0.0%	0.0%
Proceeds from borrowing	0.0%	0.0%	0.0%	0.0%	0.0%
Totals	100.0%	100.0%	100.0%	100.0%	100.0%

** Government transfers, investment income & actuarial on debt*

Objectives:

- Ensure that the Village's services are sustained over the long-term by providing for core operations and future asset/infrastructure replacements and renewals.

Policy:

- Build up reserves to optimal levels through property tax and utility user fee increases (for water and the waste & recycle depot).
- Minimize external borrowing and create internal reserve sources of funding and financing.

B. Distribution of Municipal Property Taxes Across Property Classes

Over the term of the financial plan the distribution of municipal property taxes across the property tax classes is expected to be similar to 2022, as follows:

Distribution of 2022 Property Taxes

Property Classes	Property Tax Dollars Raised	% of Total Property Taxation
1 Residential	\$952,370	98.6%
2 Utilities	\$1,112	0.1%
3 Supportive Housing	\$0	0.0%
4 Major Industry	\$0	0.0%
5 Light Industry	\$0	0.0%
6 Business/Other	\$170	0.0%
7 Managed Forest Land	\$0	0.0%
8 Recreation/Non-Profit	\$13,019	1.3%
9 Farm	\$0	0.0%
Totals	\$966,671	100.0%

Objective:

- Maintain a consistent, proportionate relationship in the sharing of the tax burden amongst the tax classes.

Policy:

- Set tax rates in accordance with the tax class multiples set by the Province of BC.

C. Permissive Tax Exemptions

Objective:

- Recognize the contributions of non-profit organizations and groups which provide services and activities for the Community.

Policy:

- Full or partial permissive tax exemptions will be considered to encourage activities or services that:
 - are consistent with the quality of life (economic, social, and cultural) objectives of the Village;
 - provide direct access and benefit to the community, and
 - would otherwise be provided by the Village.
- Council Policy 176 provides additional details and requirements for Permissive Tax Exemptions.



COUNCIL REPORT

Date: February 21, 2023

From: Ken Bjorgaard, Financial Consultant

Subject: Village of Belcarra Fees and Charges Bylaw

Recommendation:

That “Village of Belcarra Fees and Charges Bylaw No. 517, 2018 Amendment Bylaw No. 607, 2023” be read a first and second time.

Purpose:

The purpose of this report is to present “Village of Belcarra Fees and Charges Bylaw No. 517, 2018 Amendment Bylaw No. 607, 2023” (Bylaw) for first and second readings. This Bylaw is an amendment to “Village of Belcarra Fees and Charges Bylaw No. 517, 2018” and reflects the results of Council's budget direction to implement a 10% increase in water rates (for those connected and not connected to the water system) and a 12% increase in waste and recycle depot (WARD) rates for 2023.

Background:

The Bylaw attached hereto for Council's consideration reflects the above noted Council direction. As bylaws cannot be applied retroactively, the Bylaw will take effect on July 1, 2023. Thus, the existing fees and charges bylaw will be effective until June 30, 2023 at which time the new Bylaw rates will apply. In order to ensure that the full-year 2023 rates charged on tax notices reflect the full annual increases approved by Council the Bylaw rates have been adjusted accordingly. It is recommended that this Bylaw undergo first two readings on February 21, 2023. Third reading is scheduled for March 6, 2023 and adoption on March 27, 2023, the same as the Village's 2023 – 2027 Financial Plan bylaw, as these rate increases are reflected in the Village's Financial Plan bylaw.

Appendix A: Village of Belcarra Fees and Charges Bylaw No. 517, 2018 Amendment Bylaw No. 607, 2023



VILLAGE OF BELCARRA
Fees and Charges Bylaw No. 517, 2018
Amendment Bylaw No. 607, 2023



A bylaw to amend fees and charges for services

WHEREAS the Community Charter enables a local government to amend its bylaws from time to time;

AND WHEREAS the Village of Belcarra Council has deemed it necessary to amend its fees and charges bylaw;

NOW THEREFORE the Village of Belcarra Council enacts as follows:

1. This Bylaw may be cited for all purposes as the "Village of Belcarra Fees and Charges Bylaw No. 517, 2018 Amendment Bylaw No. 607, 2023"
2. That "Village of Belcarra Fees and Charges Bylaw No. 517, 2018" be amended as follows:

a) In Schedule 10 "Recycling and Garbage Collection and Removal" by

replacing:

"Authorized User (per parcel of real property improved or unimproved)	\$438.00 per annum
Authorized User (non-resident)	\$438.00 per annum
Accessory Suite (additional dwelling unit)	\$438.00 per annum"

with:

"Authorized User (per parcel of real property improved or unimproved)	\$496.00 per annum
Authorized User (non-resident)	\$496.00 per annum
Accessory Suite (additional dwelling unit)	\$496.00 per annum"

b) In "Schedule 13 – Waterworks Fees" by

replacing:

"Basic Fee – Fire Protection (not connected to water system) (prorated upon date of permit for Use Fee Domestic)	\$1,066.00
Use Fee – Domestic (connected to water system) (pro-rated as of one month after the first day of the month in which the connection permit is issued)	\$1,520.00"

with:

"Basic Fee – Fire Protection (not connected to water system) (prorated upon date of permit)	\$1,098.00
Use Fee – Domestic (connected to water system) (pro-rated as of one month after the first day of the month in which the connection permit is issued)	\$1,566.00"

3. This bylaw shall take force and come into effect as of July 1, 2023.

READ A FIRST TIME on

READ A SECOND TIME

READ A THIRD TIME on

ADOPTED by the Council on

Jamie Ross
Mayor

Paula Richardson
Chief Administrative Officer

This is a certified a true copy of
Village of Belcarra Fees and Charges Bylaw No. 517, 2018 Amendment Bylaw No. 607, 2023"

Chief Administrative Officer



COUNCIL REPORT

Date: February 21, 2023

From: Ken Bjorgaard, Financial Consultant

Subject: Water Parcel Tax Amendment Bylaw

Recommendation:

That "Village of Belcarra Water Supply and Distribution Local Area Service No. 1 Parcel Tax Bylaw No. 452, 2012, Amendment Bylaw 608, 2023" be read a first and second time.

Purpose:

The purpose of this report is to present the "Village of Belcarra Water Supply and Distribution Local Area Service No. 1 Parcel Tax Bylaw No. 452, 2012, Amendment Bylaw 608, 2023" for first two readings. This Bylaw reflects the Village of Belcarra's budget wherein the Water Parcel Tax revenue needs to be increased to pay for the increased debt servicing costs on the Village's water debt.

Background:

Based on the Village's increased annual debt servicing costs for its water debt which resulted from refinancing at a higher interest rate, the Water Parcel Tax is now being increased from \$972.91 to \$1,110.44 (an increase of \$137.53 per parcel). It is recommended that the Bylaw receive first two (2) readings on February 21, 2023. Third reading is scheduled to occur on March 6, 2023 and adoption on March 27, 2023.

Appendix A: Village of Belcarra Water Supply and Distribution Local Area Service No. 1 Parcel Tax Bylaw No. 452, 2012, Amendment Bylaw 608, 2023



VILLAGE OF BELCARRA
Water Supply and Distribution Local Area Service No. 1
Parcel Tax Bylaw No. 452, 2012
Amendment Bylaw No. 608, 2023



A bylaw to amend the Village of Belcarra's Water Parcel Tax.

WHEREAS the Village of Belcarra levied a Water Parcel Tax pursuant to section 200 of the *Community Charter*, through "Water Supply and Distribution Local Area Service No. 1 Parcel Tax Bylaw No. 452, 2012";

AND WHEREAS pursuant to "Water Supply and Distribution Local Area Service No. 1 Parcel Tax Bylaw No. 452, 2012" a Water Parcel Tax of \$972.91 per Parcel was established to recover the annual debt servicing costs (principal and interest payments) associated with a debt incurred to provide water services to a specified area of the Village of Belcarra;

AND WHEREAS the debt incurred to provide water services to a specified area of the Village of Belcarra has been refinanced by the Municipal Finance Authority at a higher interest rate resulting in increased debt servicing costs;

NOW THEREFORE, the Council of the Village of Belcarra enacts as follows:

1. This Bylaw shall be cited for all purposes as the "Village of Belcarra Water Supply and Distribution Local Area Service No. 1 Parcel Tax Bylaw No. 452, 2012, Amendment Bylaw 608, 2023".
2. "Village of Belcarra "Village of Belcarra Water Supply and Distribution Local Area Service No. 1 Parcel Tax Bylaw No. 452" is hereby amended to establish the annual Water Parcel Tax at \$1,110.44 per Parcel.

READ A FIRST TIME on

READ A SECOND TIME on

READ A THIRD TIME on

ADOPTED by the Council on

 Jamie Ross
 Mayor

 Paula Richardson
 Chief Administrative Officer

This is a certified a true copy of
 Village of Belcarra Water Supply and Distribution Local Area Service No. 1
 Parcel Tax Bylaw No. 452, 2012, Amendment Bylaw 608, 2023

 Chief Administrative Officer



COUNCIL REPORT

Date: February 21, 2023

From: Paula Richardson, Chief Administrative Officer

Subject: **UBCM Group Benefits for Elected Officials in B.C.
Village of Belcarra Council Members**

Recommendation

That staff be directed to enroll those members of Council who wish to participate in the Union of British Columbia Municipalities Group Benefits Plan for Elected Officials.

Purpose

The purpose of this report is to advise Council of the availability of an option to join the Union of BC Municipalities (UBCM) Group Benefits plan and to determine if Council members are interested in requesting and receiving the benefits package.

Background

Enrollment timelines for the Union of BC Municipalities Benefits Plan (the Plan) necessitates consideration by Council of the provision of health benefits to elected officials before February 28, 2023. Benefits are offered to Elected Officials under the following criteria:

- Enrollment for benefits must be within four months of appointment to council (deadline for enrollment is February 28, 2023)
- Enrollment must be for the full term of office
- For the optional life insurance coverage, only one multiple can be chosen for calculating benefit premiums and coverage, and will apply to everyone in the group purchasing the optional group life coverage (non-taxable benefit paid for by Elected Officials)

Benefits covered under the Plan include:

- Dental
- Extended Health Benefit Plan (EHB)
- Non-taxable benefits (premiums are paid by Elected Officials)
 - Optional Life Insurance
 - Optional Accidental Death & Dismemberment (AD&D)
 - Employee and Family Assistance Program (EFAP)

Council is provided with the following costs for consideration:

Options		Monthly	Annual Individual Cost	Annual Group Cost (for 5 Council members)
Dental:	Single	\$ 48.55	\$582.60	\$2,913.00
	Couple	\$ 90.95	\$1,091.40	\$5,457.00
	Family	\$151.04	\$1,812.48	\$9,062.40
Extended	Single	\$ 57.39	\$688.68	\$3,443.40
Health:	Couple/Family*	\$171.73	\$2,060.76	\$10,303.80
Total Cost:	Single	\$105.94	\$1,271.28	\$6,356.40
	Couple	\$262.68	\$3,152.16	\$15,760.80
	Family	\$322.77	\$3,873.24	\$19,366.20

*same price quoted for on Extended Health for Couple or Family

Based on UBCM recommendations, staff recommend that benefit coverage be provided to all individual Elected Officials and their families, where applicable and requested, with premiums paid by the Village. Elected Officials who do not require the benefit coverage can opt out of the Plan which will decrease the cost of coverage to the Village. The EHB premiums paid by the Village are non-taxable benefits and the life insurance and AD&D premiums would be non-taxable as well, as they are paid for by Council members.

The actual costs to the Village will be dependent on enrollment by Elected Officials, therefore, a range is provided showing the individual cost in each category and the full cost of enrollment should all choose to participate. The Village's 2023 budget would absorb any cost impact associated with this decision. Premiums are subject to minor inflationary adjustments over the term.

As the medical benefits are not considered remuneration, an amendment to the Council Remuneration Bylaw is not required.

The offering of employer paid benefit coverage to elected officials can also serve to attract and retain Council candidates now and in the future.



COUNCIL REPORT

Date: February 21, 2023

From: Stewart Novak, Public Works and Emergency Preparedness Coordinator

Subject: **Marine Avenue Road Rehabilitation, Engineering Quote**

Recommendation

That Council approve the quote submitted by ISL Engineering in the sum of \$19,515.00 plus applicable taxes for work relating to the 3700 block Marine Avenue Road rehabilitation.

Purpose

To maintain and rehabilitate roads within the Village, and according to priorities set out in the 2017 Urban Systems Road study and capital budgeting.

Background

Consistent with Belcarra's 5-year capital planning and as recommended in the Urban Systems road study that identified the road deficiencies and generally prioritized the issues.

Urban Systems identified Marine Avenue from Kelly Road to Young Road as needing significant base repairs and recommended that the work be completed within 3 to 5 years.

Marine Avenue Inventory-Condition-Assessment as per Urban Systems Study:

Street	From	To	Rehabilitation Work	Shoulder Maintenance	Base Repair
Marine Ave	Kelly Rd	Young Rd	Spot Repair	None	Significant base repair
Marine Ave	Kelly Rd	Young Rd	Spot Repair or reconstruct	None	Significant base repair
Marine Ave	Kelly Rd	Young Rd	Spot Repair or reconstruct	None	Significant base repair
Marine Ave	Kelly Rd	Young Rd	Spot Repair or reconstruct	None	Significant base repair
Marine Ave	Young Rd	South Cul-de-sac	Spot Repair or reconstruct	Shoulder repair	Significant base repair

Timing for Roadwork Based on Condition



Since the 2017 road study staff have observed that road sluffing continues to progress to the extent that it may become a concern to underlying infrastructure.

In 2020 Belcarra staff began the initial phase of capital planning and used the information from the existing studies as well as visually verifying the concerns prior to entering the capital project funding into Belcarra's 5-year capital plan.

As this capital budget item is now due in 2023, this report provides a quote from ISL Engineering to develop a detailed design plan for the Marine Avenue road rehabilitation project.

The scope of ISL Engineering's work includes:

- Project initiation, kick-off meeting, review of background information and initial site visits.
- Topographical survey (Maple Surveys).
- Subsurface exploration along Marine Avenue (Geotechnical allowance up to \$10,000.00 included).
- Preliminary design.
- Detailed design.
- Cost estimates

ISL Engineering's work will provide detailed design and cost estimates of the required road work as dictated by the geotechnical surveys conducted along Marine Avenue.

ISL Engineering service fees are consistent with the five-year master agreement signed in January 2020.

The project will be funded from the 2023 Marine Avenue capital budget.

Thurber Engineering was hired in 2018 to conduct a geotechnical study along Marine Avenue and the information from that study was forwarded to ISL Engineering for review, however it is expected that additional geotechnical work will be required in specific locations along Marine Avenue prior to formalizing a road rehabilitation work plan.

Appendix A: Road Assessment Map

Appendix B: Road Corridor Assessment Report, Village of Belcarra, November 17, 2017

APPENDIX A



Road Assessment

Road Surface with Distresses Indicating Potential Base Failure

- Legend
- Significant base repair
 - Moderate base repair
 - Spot base repair
 - Monitor
 - Not found

The accuracy & completeness of information shown on this drawing is not guaranteed. It will be the responsibility of the user of the information shown on this drawing to locate & establish the precise location of all existing information whether shown or not.



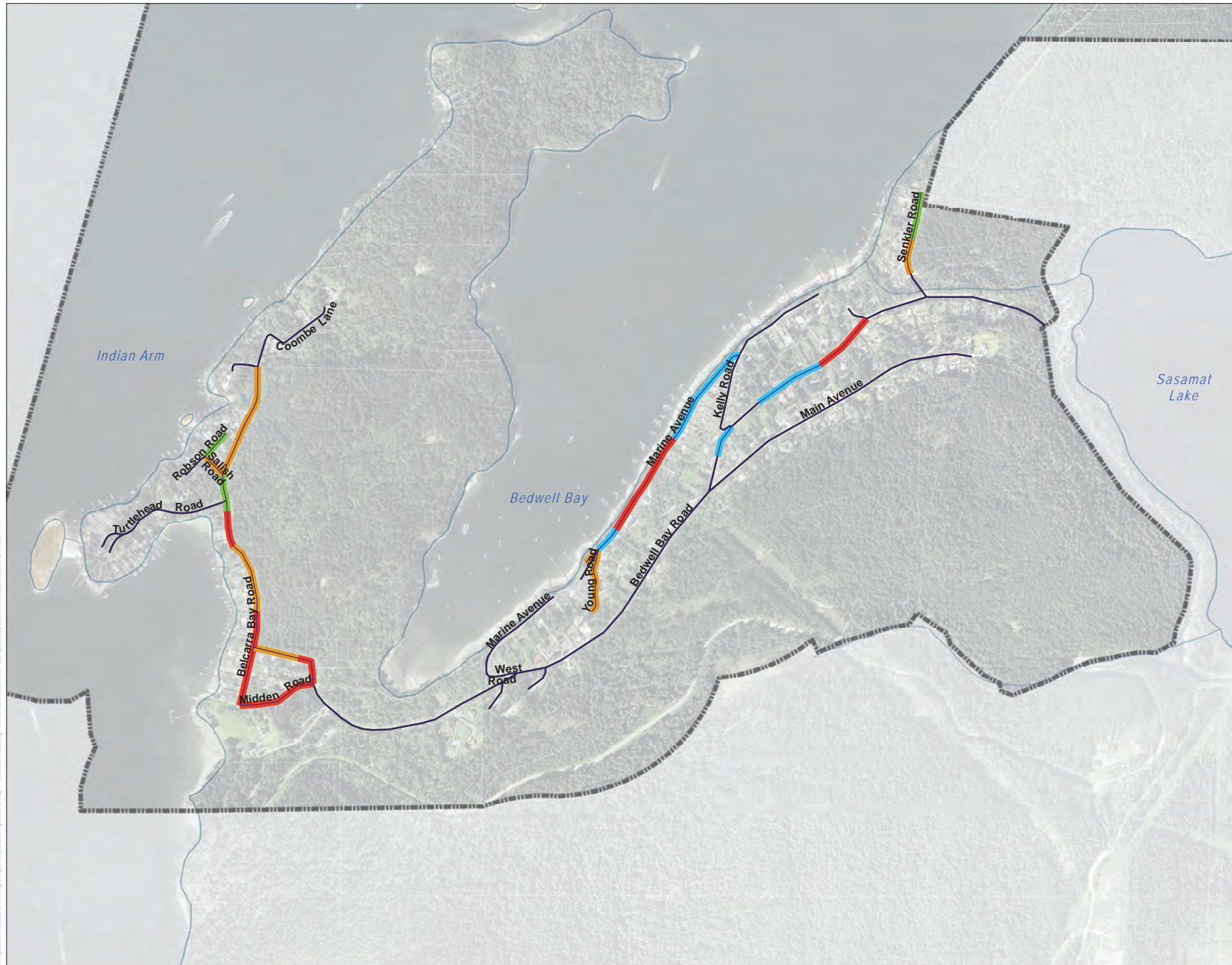
Coordinate System:
NAD 1983 UTM Zone 10N

Data Sources:
National Road Network - Geobase.ca
Orthophoto: Port Moody
Cadastral: Integrated Cadastral Information Society

Project #: 4204.0002.01
Author: WT
Checked: JW
Status: A
Revision: A
Date: 2017 / 10 / 18

URBAN
systems

MAP 4





ROAD CORRIDOR ASSESSMENT REPORT VILLAGE OF BELCARRA

November 1, 2017



Road Corridor Assessment - Village of Belcarra

4204.0002.01

550 – 1090 Homer Street
Vancouver, BC V6B 2W9 | T: 604.235.1701

Contact: John Weninger, P. Eng.

T: 604-235-1701
jweninger@urbansystems.ca

urbansystems.ca

This report is prepared for the sole use of the Village of Belcarra. No representations of any kind are made by Urban Systems Ltd. or its employees to any party with whom Urban Systems Ltd. does not have a contract. Copyright 2017.



Contents

INTRODUCTION.....1

BACKGROUND3

ROAD CORRIDOR ASSESSMENT5

ASSESSMENT FINDINGS.....17

COST ESTIMATES FOR PRIORITIZED ROADS30

SUMMARY OF RECOMMENDATIONS34

APPENDICES:

- Appendix A – Maps
- Appendix B – Cost Estimates



Introduction

This report provides the rationale for and findings of a Road Corridor Assessment (RCA) designed to determine the existing field conditions and urgency of investment in the Village's roadway assets. The impetus for this RCA is to inform the development of the Village's short, medium and long-term capital plans.



The RCA identifies and assesses the functional and physical characteristics of the Village's roadway corridors through standardized visual inspection. This is done to develop a surface condition rating as well as take stock of other roadway components that directly affect the condition and lifespan of the road surface and cost of roadway improvements (maintenance, resurface, reconstruction). In addition to the road surface review, the RCA included an inventory and general assessment of the curb, road-side barriers, retaining walls and bus stops which was of interest to Village staff.

A surface condition review does not provide definitive road structural strength assessments as can be provided through load deflection testing. It also does not provide more detailed road structure composition information that could be determined through core sampling or test pits. However, surface condition provide an indication of underlying problems. It therefore can



provide a general indication of road condition and, if assessments are completed over time, the rate of deterioration.

The inspection information is suitable for informing the Village's annual road operations and maintenance programs and to refine the Roads Asset Management Program that the Village undertook in early 2017. Road renewal and improvement needs, priorities, and options can then be identified by Village staff and Council, and affordable levels of service and risk can be determined. More detailed geotechnical and road structure investigations may be warranted in some cases to help determine if alternative road rehabilitation methods would be appropriate i.e. full depth reclamation (pulverizing) or full depth reconstruction as part of design.

The roadway corridor assessment examined the roadway characteristics which included: road surface condition, road surface type of defect, safety concerns and confirmed most cross-section components.



Background

Pavements will typically deteriorate over time at an ever-increasing rate in the absence of timely maintenance and rehabilitation actions as shown in **Figure 1**. As deterioration increases, so do the associated costs to repair them. Investments in effective and systematic maintenance and rehabilitation can slow or reverse this deterioration, extending the service life of the road and reducing life cycle costs.

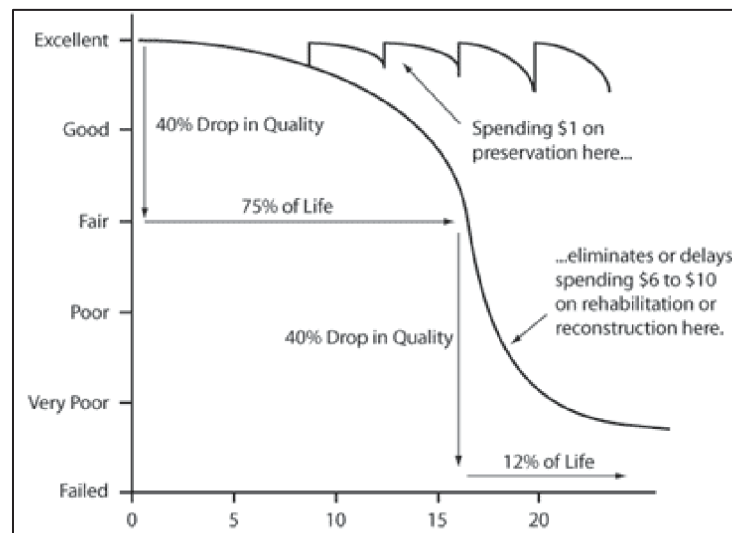


Figure 1: Typical Pavement Deterioration Curve

A typical pavement surface with a suitable base structure will deteriorate slowly for the first 75% of its life, after which it will lose the balance of its life very quickly. When the road surface is repaired regularly with maintenance interventions such as patching, crack sealing and shoulder repair, the useful life of the pavement can be extended dramatically. When it is no longer viable to extend the pavement life with maintenance, the road can be re-surfaced. Both of these timely actions (maintenance and resurfacing) can defer expensive re-construction, which includes the repair of the road base. It may be possible to get 3 or more resurfacings before reconstruction on a well-maintained and well-constructed roadway, depending on the amount of traffic and load on the roads. The road base provides the road strength, while the road surface provides a smooth, skid resistant waterproofing.

The Village can reduce its long-term road repair and reconstruction costs by proactively managing the life cycle of the pavement surface. Figure 2 provides a generalized example of the financial benefits of being proactive when considering a theoretical 1.0 kilometer of



roadway that is 7.0 meters wide. This example shows how the Uniform Annual Equivalent Cost savings is a factor of 2.6. To put this in monetary terms, for every \$1 million spent on the road in proactive road maintenance and surface repairs, a community could save in the order of \$1.6 million. Results will vary based upon pavement design, construction quality, weather, traffic loading and drainage.

Well Maintained Road			Poorly Maintained Road		
Treatment	Age	Cost	Treatment	Age	Cost
Construction	0	\$ 700,000	Construction	0	\$ 700,000
Crack Seal	5	\$ 2,000	Crack Seal	10	\$ 5,000
Crack Seal & Patch	10	\$ 3,000	Re-construct	20	\$ 700,000
Resurface	15	\$ 300,000	Total		\$ 1,405,000
Crack Seal	20	\$ 2,000	UAEC		\$ 70,250
Crack Seal & Patch	25	\$ 3,000			
Resurface	30	\$ 300,000			
Crack Seal	35	\$ 2,000			
Crack Seal & Patch	40	\$ 3,000			
Resurface	45	\$ 300,000			
Crack Seal	50	\$ 2,000			
Crack Seal & Patch	55	\$ 3,000			
Re-construct	60				
Total		\$ 1,620,000			
UAEC		\$ 27,000			

Cost Saving Ratio	
$\$70,250 / \$27,000 = 2.6$	

UAEC: Uniform Annual Equivalent Cost

Figure 2: Potential Life Cycle Cost Savings

The Village of Belcarra has approximately 8.4km of roads which made a visual assessment possible over a reasonable timeframe. The combination of standardized fieldwork, detailed inspection forms, photos and videos of each road segment allowed for site specific review and analysis for each road segment.

Recent cost estimates were provided by Belcarra staff and Ron Beesley, P. Eng of H.Y. Engineering Ltd. who has worked extensively with the Village providing engineering services over the past number of years. In addition, a 35% contingency is added in the cost estimates.

Note that assumptions should be updated over time as new information becomes available.



Road Corridor Assessment

The field assessment was conducted in late July 2017 and included all 8.4km of the Village owned roads as identified by Village staff. Each of the Village's roads segments were divided into 100 meter sections to facilitate the detailed assessment and documentation. The roads included in the RCA are shown in red on **Figure 3**.



Figure 3: Extent of 2017 Road Corridor Assessment

ROAD CORRIDOR INVENTORY

Included in the inventory was the physical dimensions and materials of the roadway components including the road surface, bike lanes, road shoulders, curbs, road-side barriers, retaining walls and bus stops.

As part of the assessment, the dimensions of the paved lanes and shoulders were measured and recorded. The road corridor dimensions are useful for calculating costs for rehabilitation efforts.



ROAD CORRIDOR RATINGS

Included in the condition assessment was the road distresses, condition of the existing crack sealing and patches. The distresses and condition were recorded using the Ministry of Transportation and Infrastructure Construction Branch's (MOTI) Pavement Surface Condition Rating Manual – Forth Edition as a guide.

http://www.th.gov.bc.ca/publications/const_maint/2012_pavement.pdf

From the assessment, we noted the recommended works to sustain or rehabilitate each road segment.

Road Surface

Pavement deteriorates for a broad number of reasons such as materials, thickness, base course, construction quality, design, loading, drainage, weather, and maintenance levels. This deterioration is exhibited through a variety of surface distresses. The Village's RCA included recording both the severity and density of each distress type observed. **Table 1** lists each distress type and provides a description of each severity level.

Table 1: Pavement Distress Rating System – Severity Levels

Pavement Distress Rating System – Severity Levels			
Distress Type	Low Severity	Moderate Severity	High Severity
Longitudinal Wheel Path Cracking (LWP)	Single cracks with no spalling; mean unsealed crack width < 5mm	Single or multiple cracks; moderate spalling; mean unsealed crack width 5-20mm	Single or multiple cracks; severe spalling; mean unsealed crack width >20mm; alligator
Longitudinal Joint Cracking (LJC)	Single cracks with no spalling; mean unsealed crack width < 5mm	Single or multiple cracks; moderate spalling; mean unsealed crack width 5-20mm	Single or multiple cracks; severe spalling; mean unsealed crack width >20mm; alligator
Pavement Edge Cracking (PEC)	Single cracks with no spalling; mean unsealed crack width < 5mm	Single or multiple cracks; moderate spalling; mean unsealed crack width 5-20mm	Single or multiple cracks; severe spalling; mean unsealed crack width >20mm; alligator
Transverse Cracking (TC)	Single cracks with no spalling; mean unsealed crack width < 5mm	Single or multiple cracks; moderate spalling; mean unsealed crack width 5-20mm	Single or multiple cracks; severe spalling; mean unsealed crack width >20mm; alligator
Meandering Longitudinal Cracking (MLC)	Single cracks with no spalling; mean unsealed crack width < 5mm	Single or multiple cracks; moderate spalling; mean unsealed crack width 5-20mm	Single or multiple cracks; severe spalling; mean unsealed crack width >20mm; alligator
Alligator Cracking (AC)	Not rated	Interconnected cracks forming a complete block pattern; slight spalling and no pumping	Interconnected cracks forming a complete block pattern, moderate to severe spalling, pieces may move and pumping may exist
Rutting (RUT)	Less than 10mm	10 to 20mm	Greater than 20mm
Shoving (SHV)	Barely noticeable to noticeable	Rough ride	Very rough ride
Distortion (DST)	Not rated	Noticeable swaying motion; good car control	Fair to Poor car control
Bleeding (BLD)	Not rated	Distinctive appearance with free excess asphalt	Free asphalt gives pavement surface a wet look; tire marks are evident
Potholes (POT)	Less than 25mm deep and greater than 175cm ² in area. (~15cm Ø)	25 to 50mm deep and greater than 175cm ² in area. (~15cm Ø)	Greater than 50mm deep and greater than 175cm ² in area. (~15cm Ø)
Ravelling (RAV)	Not rated	Aggregate and/or binder worn away; surface texture rough and pitted; loose particles exist	Aggregate and/or binder worn away; surface texture is very rough and pitted

Source: MOTI Pavement Surface Condition Rating Manual – Forth Edition

Table 2 describes the density rating for each distress type.






Table 2: Pavement Distress Rating System – Density Levels




Pavement Distress Rating System – Density Levels							
Distress Type	Units	None	Few	Intermittent	Frequent	Extensive	Throughout
Longitudinal Wheel Path Cracking (LWP)	Length	0%	< 10%	10-20%	20-50%	50-80%	80-100%
Longitudinal Joint Cracking (LJC)	Length	0%	< 10%	10-20%	20-50%	50-80%	80-100%
Pavement Edge Cracking (PEC)	Length	0%	< 10%	10-20%	20-50%	50-80%	80-100%
Transverse Cracking (TC)	Number	0	1-2	3-4	5-7	8-10	>10
Meandering Longitudinal Cracking (MLC)	Length	0%	< 10%	10-20%	20-50%	50-80%	80-100%
Alligator Cracking (AC)	Area	0%	< 10%	10-20%	20-50%	50-80%	80-100%
Rutting (RUT)	Length	0%	< 10%	10-20%	20-50%	50-80%	80-100%
Shoving (SHV)	Length	0%	< 10%	10-20%	20-50%	50-80%	80-100%
Distortion (DST)	Length	0%	< 10%	10-20%	20-50%	50-80%	80-100%
Bleeding (BLD)	Length	0%	< 10%	10-20%	20-50%	50-80%	80-100%
Potholes (POT)	Number	0	1-2	3-4	5-6	7-9	>10
Ravelling (RAV)	Length	0%	< 10%	10-20%	20-50%	50-80%	80-100%

Source: MOTI Pavement Surface Condition Rating Manual – Forth Edition


Table 3 on the next page provides a photo and description of each of the above road surface distresses. The photos are of Belcarra roads and the description and cause of the distress are from the MOTI Pavement Surface Condition Rating Manual – Forth Edition.


Table 3: Road Surface Distresses & Descriptions



DISTRESS	IMAGE	CAUSE
Longitudinal Wheel Path Cracking (LWP)		Cracks which follow a course predominantly parallel to the pavement centre line and are located at or near the centre of the wheel path. LWP cracking indicate fatigue failure from heavy vehicle loads. Cracks within one foot of the edge are caused by insufficient shoulder support, poor drainage, or frost action.
Longitudinal Joint Cracking (LJ)		Cracks running in the direction of traffic are longitudinal cracks. Centre line or lane cracks are caused by poor construction of longitudinal joint, frost action on adjacent lanes with variable granular depths.
Pavement Edge Cracking (PEC)		Cracks which occur parallel to and within 0.6 metres of the inside and/or outside of the fog line. Cracks may be crescent shaped cracks or other fairly consistent cracks which intersect the pavement edge. Possible causes include: Frost action, inadequate pavement structural support at the pavement edge and/or excessive traffic loading, poor drainage at the pavement edge and shoulder, inadequate pavement width forces traffic too close to the pavement edge.

DISTRESS	IMAGE	CAUSE
Transverse Cracking (TC)		<p>Cracks that are predominantly perpendicular to the pavement centre line and extend fully or partially across the roadway.</p> <p>Possible causes include: surface shrinkage caused by low temperatures, temperature susceptibility of the asphalt cement binder in asphalt mixes, frost action, reflection cracks and hardening of the asphalt with aging</p>
Meandering Longitudinal Cracking (MLJ)		<p>Cracks which wander from edge to edge of the pavement or run parallel to the centre line, situated near the middle of the lane. MLJ cracks are usually single cracks, but secondary cracks can develop in areas where transverse thermal cracks also exist.</p> <p>Possible causes include: Frost action with greater heave at the pavement centre than at the edges. This is more prevalent in mixes where asphalt stripping is extensive. Faulty construction equipment can cause weak planes in the mix, which can fail from thermal shrinkage</p>
Alligator Cracking (AC)		<p>Cracks which form a network of multi-sided blocks resembling the skin of an alligator. Block size can range in size which indicates the depth of failure taking place. The pattern of cracking is usually longitudinal, originating in the wheel paths, but can occur transversely due to frost heaves or settlement and also along the centre line on narrow two-lane roads.</p>



DISTRESS	IMAGE	CAUSE
		<p>Possible causes include: areas subjected to repeated traffic loadings. Insufficient bearing support due to poor quality base materials or saturated base with poor drainage, stiff or brittle asphalt mixes at cold temperatures.</p>
Rutting (RUT)		<p>Longitudinal depressions left in the wheel paths after repeated loadings, combined with sideways shoving of the pavement material.</p> <p>Possible causes include: Poorly compacted structural layers, Heavy loadings of saturated unstable granular bases/sub-bases during spring thaw periods, unstable asphalt mixes due to high temperature or low binder viscosity, inadequate lateral support from unstable shoulder materials, permanent deformation of an overstressed subgrade</p>
Shoving (SHV)	<div style="border: 1px solid black; padding: 5px; text-align: center;">None observed in Belcarra</div>	<p>Longitudinal displacement of a localized area of the pavement surface generally caused by braking or accelerating vehicles and usually located on hills, curves or intersections.</p> <p>Possible causes include: Heavy traffic on steep downgrades or upgrades, low stability asphalt mix, lack of bond in asphalt surface and underlying layer, unstable granular base.</p>

DISTRESS	IMAGE	CAUSE
Distortion (DST)		<p>Any deviation of the pavement surface from its original shape other than that described for shoving and rutting. Generally, distortions result from settlement, slope failure, volume changes due to moisture changes and to frost heaving, and from residual effects of frost heaving accumulating after each season.</p> <p>Possible causes include: Differential frost heaves in poorly drained cuts, transitions and at pavement edges or centre. reverse differential frost heave at culverts, differential settlement of subgrade or base materials, lack of subgrade support, embankment slope failure.</p>
Bleeding (BLD)	None observed in Belcarra	<p>Excess bituminous binder on the pavement surface can create a shiny, glass-like, reflective surface that may be tacky to the touch. Bleeding quite often occurs in the wheel paths.</p> <p>Possible causes include: Mix design deficiencies where too high an asphalt content relative to voids results in excess asphalt forced to the surface by traffic, especially on hot days, paving over surfaces with severe bleeding or the application of a heavy prime or tack coat under a new pavement layer may result in excess primer bleeding through the pavement surface over a period of time, poor construction of surface seal coats.</p>

DISTRESS	IMAGE	CAUSE
Potholes (POT)		<p>Bowl-shaped holes of various sizes in the pavement surface.</p> <p>Possible causes include: Thin spot in the asphalt layer, localized drainage problems such as water infiltration through poorly bonded pavement structural layers or segregated spots in the asphalt mix where coarse patches allow intrusion of water, asphalt mix design deficiencies.</p>
Ravelling (RAV)		<p>The progressive loss of the pavement material from the surface downward, leaving a rough surface, vulnerable to weather deterioration.</p> <p>Possible causes include: poor adhesion of aggregates due to insufficient asphalt content, clay-coated aggregate, use of wet aggregates or stripping due to water action, fracture of aggregate particles by heavy loads or natural causes, the unbound particles are then removed by traffic, reducing the depth of the asphalt, poor compaction permits infiltration of water and salts which promote asphalt stripping, segregated mix placed during construction, aging and weathering.</p>
Tree Roots	<div data-bbox="454 1537 849 1617">None observed in Belcarra</div>	<p>Surface failures caused by tree roots. Caused by tree roots growing between road base and asphalt surface. The force from the tree roots causes the localized swelling and cracking of the road surface.</p>



DISTRESS	IMAGE	CAUSE
Trench Joint		Trench joint cracking occurs where there has been recent disturbance of the road surface and road base for the installation of infrastructure or for other purposes. If the fill material is not fully compacted or if compaction does not match the original material, it can cause cracking at the joint or surrounding area near the trench cut. Cracking is often at the edges for the original surface to crack and fail.

Photos of Village of Belcarra roads taken by Urban Systems – July 2017

Source: MOTI Pavement Surface Condition Rating Manual – Forth Edition

Crack Sealing and Patches

In addition to the surface distresses noted above, the condition of crack sealing and patches were reviewed. The crack sealing and patch conditions were recorded as Level 1-3 described below:

Crack Sealing Condition:

- Level 1 - < 30% of cracks are sealed
- Level 2 - 30% to 60% of cracks are sealed
- Level 3 - > 60% of cracks are sealed

Patch Condition:

- Level 1 - Few small localized patches
- Level 2 - Several larger patches
- Level 3- Full lane patching

For patch condition a condition rating was also added to take stock of the condition of the patches.

- 1 = Poor; patch ineffective and should be repaired as soon as possible
- 2 = Fair; some patch defects, repair if funding available
- 3 = Good; patch in good condition, no repair required



Level 2 Patch - In poor condition



Inadequate crack sealing

Figure 4: Examples of the Condition of Patching and Crack Sealing

Base Failure

As part of the assessment, roads that appeared to show signs of base failure were flagged for further review. Since the RCA was a visual assessment of the road surface and associated road corridor components, we could not definitively determine if the roads have an adequate road base, the base condition or extent of any base failure. We recommend that the Village have a geotechnical assessment done in key locations to better understand the condition of the road structure prior to overlaying or reconstructing the roads.

Drainage

Drainage is a very important factor to help ensure the integrity and longevity of the Village's roads. If water can penetrate the road structure it will weaken and fail prematurely. Any water needs to be kept below the road structure with the use of open ditches, culverts or other drainage infrastructure. Please note that an evaluation of the Village's ditches and drainage infrastructure was not included as part of our scope since this work will be carried out by another engineering firm as part of the Village's stormwater planning. It is recommended that the Village request that they consider how the current drainage deficiencies will affect the lifespan of the Village's roads. Once this stormwater planning is complete, the results can be used to further refine the Village's Asset Management Program. For reference, below is the drainage condition assessment guideline from the MOTI Pavement Surface Condition Rating Manual.



Table 4: Road Corridor - Drainage Condition

ACCEPTABLE
<ul style="list-style-type: none"> ➔ Cross section and drainage are fully adequate. ➔ Concealed underground storm drains in good repair. ➔ Open ditching with no free-standing water and no silt bottom layer or obstructed culverts, etc. ➔ Open ditching with free-standing water or bull rushes in the ditch and the fill height is greater than 1.5 metres.
BORDERLINE
<ul style="list-style-type: none"> ➔ If the ditch grade line, cross-section elements and/or culvert and/or ditch capacity are somewhat below the standard that would be provided if the road and ditches were rebuilt. ➔ Roads with acceptable design characteristics, but poorly maintained ditching, requiring work to be brought up to an acceptable level. Work required should generally fall into a category that could be completed by gradual cleaning of ditches, grading of shoulder areas and minor culvert repair.
UNACCEPTABLE
<ul style="list-style-type: none"> ➔ Free-standing water in ditches, grass and other debris, requiring more than minor work to be brought up to an acceptable standard; granular washout of shoulder areas, etc. ➔ Conditions could impede safe traffic movement. ➔ Areas with lack of grade could possibly flood. ➔ Catch basins are in a very poor state of repair with obvious pavement deterioration and free-standing water. ➔ Water channels onto driven portion of road. ➔ Road drains onto adjacent occupied properties

Source: MOTI Pavement Surface Condition Rating Manual – Forth Edition



Curb, Road-Side Barriers, Retaining Walls and Bus Stops

Included in our assessment was an inventory and general condition assessment of the Village's curb, road-side barriers, retaining walls and bus stops. For each of these components, we recorded their location, material, condition and any notable deficiencies.

The general condition rating was completed using the following scale:

- 0 = Not Inspected;
- 1 = Poor; severe defects, should repair immediately;
- 2 = Fair; some defects, monitor for future repair, repair now if funding available;
- 3 = Good; some minor defects, monitor for future repair; and
- 4 = Excellent; no substantial defect, no repair required.

Specific issues that were found during the road assessment were recorded to provide further detail about the distresses, hazards, potential causes and recommended works to sustain, or rehabilitate these assets.

Reporting Template

During the field inspections, a road inspection database was populated. Each road was also videoed with a high definition camera and distresses found were photographed. The photos and videos are organized by road segment for further reference. The database of the field inspections was compiled in GIS format but provided in MS Excel for ease of reference. This table is attached to this report. The found deficiencies inform an overall rating of the severity for each road segment.

All the collected GIS information, photos and videos are provided along with this report for future reference.



Assessment Findings

ROAD SURFACE

Although there are high severity deficiencies and roads needing attention, it should be noted that many of the road surface distresses can be addressed within a road maintenance program. As an initial overview, roads with high severity and high density are likely candidates for reconstruction whereas roads with high severity but lower density are in many cases, candidates for spot repair and priority patching. The severity of distress observed during the RCA is summarized by length in **Table 5**.

Table 5: Surface Distress Overview

Distresses Severity	Severity Score	Length (m)	Percentage
High	> 1000	747	8.9%
Moderate to High	> 200 & < 1000	628	7.5%
Moderate	> 101 & < 200	671	8%
Low to Moderate	> 10 & < 100	1179	14%
Low	< 11	5187	61.6%

A severity score was assigned to each road as a way of summarizing the number and severity of the distresses. The score applies a weighting factor to those distresses that are most severe. The score is simply used to help highlight candidate roads but then the actual field inspection reports were considered in more detail to help recommend maintenance, repair and reconstruction works.

Please note that the severity colour coding matches Map 2 – Distress Severity Ranking in **Appendix A**.

We do not recommend the approach of only investing in full reconstruction works first as it would defer maintenance works such as crack sealing and patching, shoulder as well as pothole repair. It would also delay investment in the overlay of some roads that would benefit from rehabilitation prior to major base failure occurring.



CRACK SEALING AND MINOR PATCHING

Surface cracking in the road surface allows water to penetrate and weaken the road structure. Once the road structure is weakened, the deterioration of the road surface increases. To prevent water from entering the road structure it is recommended that the Village use a portion of the annual roads budget to establish a crack sealing and minor patching program. In addition, as highlighted in the EXP Geotechnical Advice and Comments – Proposed Overlay of Bedwell Bay Road Report, it is good practice to fill existing cracks in the pavement before overlaying to help seal off moisture and retard reflective cracking.

We recommend an increased focus on crack sealing and patching until the Village has addressed most of the high and moderate severity cracking. Crack sealing should include the trench joint edges along the new and old asphalt from the recent water system installation.

In addition to the conventional linear crack sealing, SealTec and likely other contractors, can use spray patching for areas with alligator cracking, pothole repair and other applications. Spray patching may be an cost saving measure worth considering as a means to defer more expensive rehabilitation efforts. To determine the costs for the Village's crack sealing we contacted SealTec Industries and were provided the following costs:

Table 6: Cost Information Provided by SealTec Industries

Spray Patching	DESCRIPTION	UNIT	SealTec Costs
Cost per Litre	based on 5000 litres or more project	litre	\$8.80
Cost per square metre		Sq.m.	\$12.36
Cost per lineal meter		l.m.	\$5.06
Cost per hour		hr	\$440.00
Traffic control		hr	\$150.00
Crack Sealing	DESCRIPTION	UNIT	SealTec Costs
Hot applied sealant (per MMCD)	Based on 10,000m or more project	l.m.	\$1.55

Based on the above costs and the approximate extent of surface cracking and trench patches, we recommend the Village establish a budget allowance for crack sealing and spray patching. The Village should also include an allowance for patching of base failures that arise prior to reconstruction or resurfacing as well as repair of some existing trench patches.



Table 7: Crack Sealing and Minor Patching

Method	Quantity	Unit	Unit Cost	Totals
Spray patching for alligator cracks & potholes	2650	Sq.m.	\$12.36	\$32,755
Crack Sealing Program	4	days	5,000	\$20,000
Total				\$52,755

Please note that SealTec's costs are based on a minimum call out is \$3500 plus \$150/hour for traffic control. At this rate it is recommended that the Village allocate \$10,000/year for the next couple of years to address the pavement cracking and alligator cracking in areas not scheduled for reclamation or reconstruction. The goal is to slow the deterioration of the existing roads. Future crack sealing can be completed as necessary. In the priority capital planning we have included \$50,000 for crack sealing and spray patching.

BASE FAILURE

During the RCA, roads that appeared to show signs of base failure were flagged for further review. Map 4 – Road Surface with Distresses Indicating Potential Base Failure in Appendix A shows the result of the RCA and the roads with distresses indicating potential base failure. The distresses noted include:

- Moderate to severe longitudinal wheel path cracking;
- Moderate to severe pavement edge cracking with distortion;
- Roads with moderate to high severity rutting and distortion; and
- Alligator cracking.

Table 8: Roads with Potential Base Failure

Distress Severity	Length (m)	Percentage
Significant Base Repair Suspected	1,169m	13.9%
Moderate Base Repair Suspected	1,032m	12.2%
Spot Base Repair Suspected	289m	3.4%
Roads to monitor for base movement	668m	8%
Roads without base failure indications	5,255m	62.5%



We recommend the Village undertake the following to better understand the existence and condition of the road base prior to undertaking significant roadwork projects:

- Obtain photos and any geotechnical reporting from the recent water system construction to see if they show the structure of the road as an indication of depth of road base.
- Have a geotechnical assessment done (core testing or other load testing) along roads with high density distresses indicating potential base failure. The testing should be focused to the roads that are due for rehabilitation over the next 1-5 years. This testing will help determine the existence, condition and structural integrity of the existing road base as well as the condition of the existing asphalt material. This testing will help inform which rehabilitation method will be more appropriate including mill and overlay with base repair, reclamation or if full reconstruction is necessary. This testing will provide the Village with greater confidence that the best result will be achieved for the money spent. To have this testing done, we recommend the Village allocate up to \$25,000 to cover the cost of this geotechnical assessment.
- If the road base is near adequate, the Village may be able to have the road reclaimed (pulverized). Reclaiming includes pulverizing the existing road surface and mixing with additional crushed gravel and magnesium chloride forming a strengthened road base. The road is then graded and repaved. If reclaiming is determined to be appropriate, it could be a significant cost savings and enable the Village to extend their roads program within the existing annual budget.

ROAD SHOULDER CONDITION

The road shoulder maintains the structural integrity of the road by preventing the asphalt from cracking and falling apart at the sides. A shoulder also makes the road way safer for cyclists, pedestrians, vehicles and drivers by maintaining a proper slope beside the road, thus decreasing the drop-off. With some of the narrow roads in Belcarra, the road shoulder often receives direct vehicle traffic which increases the amount of deterioration. If the shoulders are not maintained, they can quickly erode with surface water and vehicle traffic.

Pavement edge cracking was one of the main road deficiencies found during the field assessment which is directly tied to the integrity of the road shoulder and road base. As part of the assessment of the road surface, we made note of shoulder deterioration and deficiencies that the Village should consider addressing to prevent further deterioration and reduce potential safety concerns.

The below photos show examples of shoulder deterioration found during our assessment:

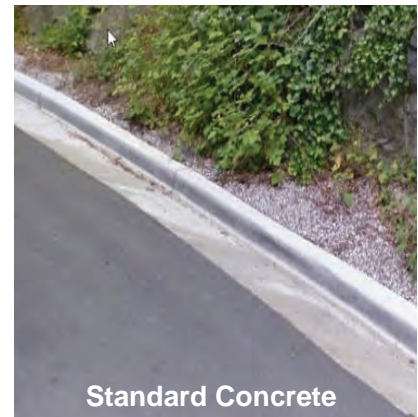


We recommend the Village ensure shouldering is included when the roads are rebuilt or resurfaced and establish a shouldering budget to address the current deficiencies and others that appear from one year to the next. This work may be able to be completed by Village staff or hired out to a local contractor with shouldering equipment that includes compaction.



CURB CONDITION

The Village of Belcarra currently has three different types of curb. These include short 50mm asphalt curb, standard 150mm asphalt curb and standard 150mm concrete curb and gutter. Please note that most of the 150mm asphalt and concrete curb appear to be quite new and in good condition. Below are photos of each curb type in Belcarra.



During the field review it was found that most of the Village's 50mm short curb is in fair to good condition. There are however, curb that have some notable deficiencies and that show some signs of deterioration. Most of the deficiencies appear to be caused by heavy vehicle traffic along tight corners and recent construction activities where the curb has been disturbed. The deterioration appears to be caused by grading during snow removal and wear from vehicles coming in contact with the curb.



Table 9: Curb Condition

Condition	Length (m)	Percentage
Poor	0	0%
Fair	617	20%
Good	1996	65%
Excellent	456	15%

Map 6 in Appendix A shows an inventory of the Village's curb.

For the Village's curb assets, we recommend the following:

- Undertake spot repairs to address curb damage from excessive vehicle impact, tight corners and residential construction;
- Replace existing curb as part of road resurfacing and rehabilitation projects;
- Add curb where necessary to direct surface water and prevent shoulder and property damage; and
- Continue to have curbing a consideration as part of the road rehabilitation program.

We provide the curb costs in the following table for reference.

Table 10: Curb Cost

Rehabilitation	Unit	Cost
50mm Asphalt Curb	m	\$25
150mm Asphalt Curb	m	\$40
Concrete Curb and Gutter	m	\$120



CONDITION OF EXISTING ROAD-SIDE BARRIERS

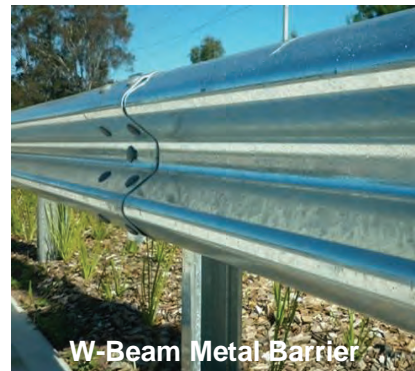
The Village has three main types of road side barriers in use. These include low concrete, standard concrete and metal W-beam road-side barriers. Below are photos of each type of road-side barrier used in Belcarra.



Low Barrier



Roadside Barrier



W-Beam Metal Barrier

During the field review, it was found that most of the Village's concrete road-side barriers are in fair to good condition with only mild corrosion, spalling and chipping. All of the W-beam road-side barriers are new in the past few years and they are in excellent condition.

Although the actual concrete barriers are in fair to good condition, there are some barriers which are currently leaning or partially buried and need to be re-seated. Re-seating the barriers requires the barrier to be removed or temporarily relocated, the road edge or shoulder reggraded and compacted and the barrier put back into place.

If the Village decides to replace existing barriers or install new barriers, they can anticipate the following costs:

Table 11: Road-side Barrier Costs

Rehabilitation	Unit	Cost
Concrete Low Barrier	m	\$75
Concrete Roadside Barrier	m	\$100
W-Beam Barrier	m	\$300

CONDITION OF RETAINING WALLS

As part of the RCA, we inventoried the retaining walls owned by the Village. Please note that we did not inventory or comment on the condition of private retaining walls. As part of the inventory, we recorded the material type, length, approximate height (above ground), the overall condition and notable deficiencies.

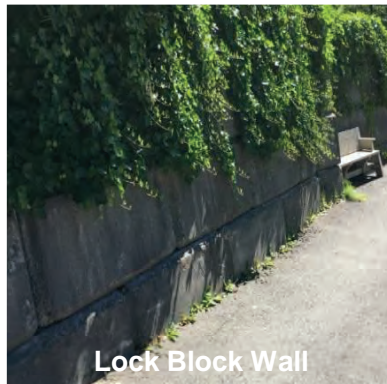


There are two main types of retaining walls in use that include gabion retaining walls and lock block retaining walls. There is also a stretch of road side barrier which is currently acting as a retaining wall along Marine Drive.

Below are photos of each type of retaining wall used in Belcarra.



Gabion Wall



Lock Block Wall



Roadside Barrier Wall

The only notable deficiencies found are on the two gabion retaining walls. The walls are at the intersection of Bedwell Bay Road and Main Avenue and the other retaining wall is at Bedwell Bay Road and Kelly Road. Both walls show signs of wire deterioration and bulging. Village staff have been maintaining the wall at Bedwell Bay Road and Main Avenue by adding wire and reseating the fill rock to keep it in place. The north-west lane of Bedwell Bay Road is built on top of the gabion wall at Bedwell Bay Road and Kelly Road and therefore this wall would have the highest impact if the wall was to further deteriorate or fail. It is our recommendation that both walls be assessed by a qualified Geotechnical Engineer to determine if there are any concern with these walls, provide recommended actions, timing and cost if any work should be considered in the next 5-10 years.

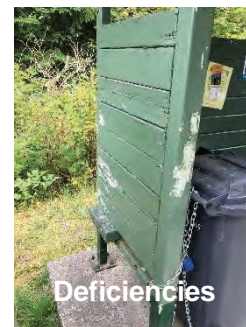




CONDITION OF BUS STOPS AND SHELTERS

As part of the RCA, we inventoried the bus stops and shelters within the Village. As part of the inventory, we recorded if the bus stop is a sign only or includes a shelter structure, the overall condition and notable deficiencies. The shelters typically have a small concrete pad, painted metal frames with painted wood bench, side panels and roof structure.

Below are photos of typical bus stops and shelters used in Belcarra.



The only notable bus stop deficiencies found during our field assessment is that a few of the shelters have a few rotting side panel and roof structure boards and the paint on the metal frame and bench is chipping and peeling. It is recommended that Village staff replace the rotting boards, sand and re-paint the frame, bench and side panels within the next year or two to prevent further deterioration. Both deficiencies should be easy addressed within the Village's regular maintenance program.

SAFETY CONCERNS

As part of the field assessment, we looked to identify specific risks, hazards and pedestrian safety concerns. This assessment includes noting potential for vehicle damage or loss of control while driving as well as cycling safety. Prior to starting the field assessment, we consulted the Village Public Works staff to get their insight and identify any safety issues they were already aware of.

There were a few safety concerns that are identified during the field assessment. The deficiencies that were of concern included:

- Potholes, road edge, corner failures and shoulder drop-offs;
- Catch basin failure;
- Steep slopes with no roadside barriers;

- A power pole within paved road surface;
- Blind corner;
- Private stair case that starts at road edge; and
- Cross-walk signage.



Figure 9: Safety Concerns Found During the RCA

Potholes, road edge, corner failures and shoulder drop-offs:

These deficiencies are a concern as they can cause drivers or cyclists to lose control of their vehicle or bicycle and cause damage. It is recommended that the Village address these deficiencies as soon as possible. Village staff could quickly eliminate the shoulder and corner drop-off issues by adding gravel and compacting the road shoulder or have a contractor come



fill and compact these areas. Adding gravel may be a fix until asphalt repair work can be scheduled to deal with failed road edge.

Catch basin failure:

There was a single catch basin drain that was found near the intersection of Marine Drive and Young Road that is significantly rusted and corroded. The grate of the catch basin is caving in and is a safety hazard for vehicles, cyclists, kids and animals. The catch basin is currently marked with a traffic delineator. It is recommended that this catch basin either be replaced or filled and capped if it is not needed at that location.

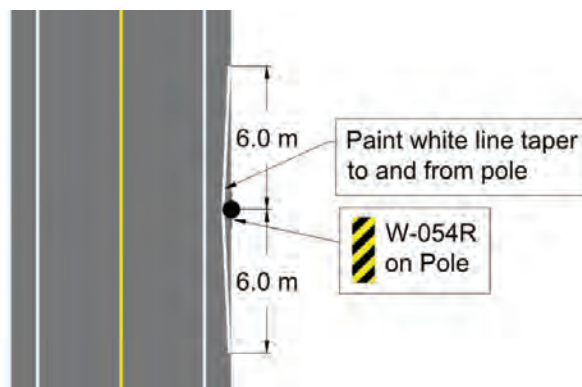
Steep slopes with no roadside barriers:

Due to the steep side slopes in Belcarra, there are several locations where roadside barriers could be added to improve safety. Particular locations to note are on Bedwell Bay Road between Main and the Village office on the steep slope section on the north-west side of the road. The below map shows the road sections where roadside barrier should be considered. In areas with steep slopes just off the road shoulder, it is recommended that Village consider W-beam roadside barrier since it can be installed without significant disturbance to the road edge or the steep slope.

A power pole within paved road surface:

In the newly paved section of Bedwell Bay Road, the contractor paved around a power pole which creates a potential hazard for vehicles and cyclists. It is recommended that the pole be relocated off the road to ensure this hazard is eliminated.

Currently there is a reflective sign on the pole but until the pole is relocated, the Village may wish to add additional paint lines to guide motorists and cyclists around the pole. Below is a graphic showing a potential layout of the barrier at this pole. The Village may wish to add additional signage in advance of this hazard. To fully scope out a solution, the Village could draw upon the services of the Municipal Insurance Association of BC (MIABC) risk management team.





Blind corner:

At Coombe Lane there is a blind corner that the Village may wish to address in the future. This is a pre-existing hazard and likely residents are used to this corner. There is signage but there is very limited visibility for vehicles, pedestrians and cyclists. The Village may want to eventually straighten this s-turn section of road and address the blind corner at the same time.

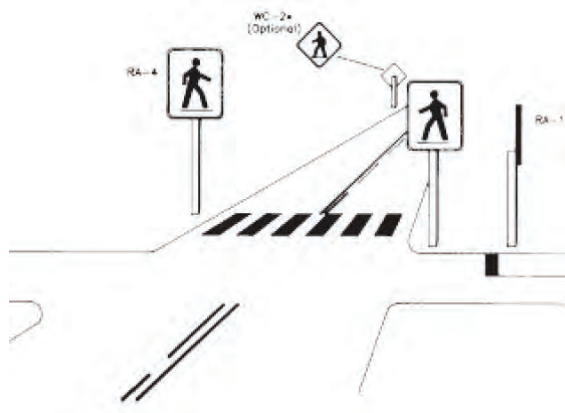
Private stair case that starts at road edge:

On Marine Drive there is a private stair case leading to a dock that starts right at the road edge. The first stair tread starts at the edge of the road. Although there is a railing that starts on the top of the stairs, it is recommended to add reflective signage to both sides of the stair handrail to ensure motorists and cyclists to do hit the stair case.

Cross-walk signage:

It was mentioned by Village staff that the current cross walk signage is yellow and not the standard white cross walk signage. In checking with the Ministry of Transportation and Infrastructure (MOTI) design guidelines for signage, it was confirmed that the standard cross walk signage is white and a yellow diamond shaped signage is typically used as an approach warning. Approach signage is optional but if installed should be mounted 65m before the cross walk given a 50km/h speed limit.

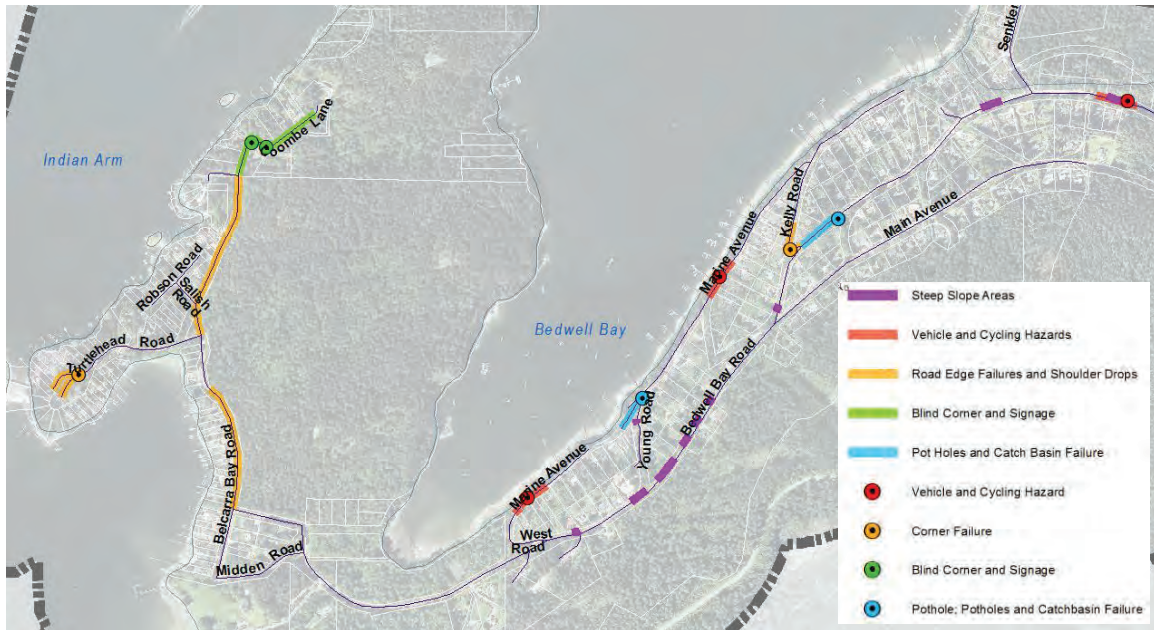
For the low cost of switching the signage to the white version, it is likely worth the cost and effort to reduce potential liability for the Village. To confirm the need to switch out the signage, the Village could draw upon the services of the Municipal Insurance Association of BC (MIABC) risk management team.





Road Corridor Assessment - Village of Belcarra

The map below shows the location of the safety concerns found during the field assessment.





Cost Estimates for Prioritized Roads

For this project, Urban Systems has provided Class C cost estimates for the identified roads that can be addressed over the next ten years. Capital works have been identified as either fully reconstruct or mill and overlay roads depending on the severity and density of distresses and indications of potential base failure. Please note that it may be possible to do full depth reclamation (pulverizing) instead of the full reconstruction which would enable the Village to save costs and extend the roads program. From our experience full depth reclamation costs approximately half of the cost of full reconstruction (if a sufficient area of road undergoes reclamation to help offset the costs of mobilizing the reclamation equipment).

For the cost estimates, we have costed the roads with high distress severity as full reconstruction projects. It may be possible to use full depth reclamation or localized, large area base repairs but the approach is best determined by a Geotechnical Engineer with the use of core samples and other testing.

KEY PERSPECTIVES FOR SCHEDULING WORKS

The following perspectives were utilized to help establish priority road works that could be accommodated within the Village's budget.

Health and Safety Concerns

Road edge and shoulder repair should be a priority focusing first on the sections with health and safety concerns including damaged corners due to heavy truck activity from new construction, pavement edge and pot holes patching and building up shoulder drops. Addressing these issues will significantly reduce hazards to vehicles and cyclists and reduce the likelihood of accelerated deterioration at these locations.

The addition of road side barrier along Bedwell Bay Road and other noted locations will increase roadway safety.

The costs for these repairs are best determined by your Public Works staff but we have provided an allowance for this work in 2018.

Crack Sealing and Minor Patching

Crack sealing and patching on all roads that are not candidates for full depth reconstruction should be considered as higher priority. The investment in that maintenance work could help avoid or delay more extensive base failure. Crack sealing and minor patching works were therefore distributed over the first five years with further investments over the ten-year period



as new cracks occur. Cost per meter for crack sealing was provided by SealTec Industries and applied to the amount of surface cracking observed during the RCA.

Rehabilitation of Local and Collector Roads

Based on our visual assessment, approximately 55% of the Village's roads are in good condition and do not currently need any surface work besides crack sealing and monitoring for surface and base deterioration.

Approximately 14% of the roads in Belcarra are suspected of requiring significant base repair when they are to be addressed. It is recommended that a geotechnical assessment be completed for the identified areas with potential base failure to determine if full-depth reclamation should be used instead of a mill and overlay with localized base repair. The addition of consistent base structure should help extend the service life for these roads. Once this assessment has been completed, the costs and the Village's 5-year plan can be updated.

The remaining 31% of roads are candidates for crack sealing, patching and mill and overlay rehabilitation with some base repair to address localized base issues. Completing the road maintenance and surface repair works as soon as possible can help defer more expensive reconstruction work for some of the road segments.

Shoulder Repair and Maintenance

We have provided an allowance for shoulder repair that will enable the Village to address many of the issue found during the RCA. It is anticipated that this work will include the grading of built-up vegetation at the pavement edge as well as other shoulder rehabilitation.

The priorities for some of the shoulder road work may be influenced by the stormwater planning work, so it is recommended to align these priority projects once they have been identified.

Coordination with Other Infrastructure Works

The general priorities provided in this report may be impacted by other capital works in the community. We understand that the Village is pursuing adding the Bedwell Bay Road, Belcarra Bay Road and Midden Road to the Major Road Network (MRN) as it is an existing transit route. If it looks like this section of road will be added to the MRN, the Village may wish to defer the road works in this area so that MRN funding can be used to help fund the rehabilitation. In addition to this section of road, drainage improvements or other roadway improvements may change road priority timings.



PROPOSED CAPITAL WORKS

Priority road works were identified using the above considerations. The recommended road works can be seen on Map 2 – Recommended Road Surface Works 2018 – 2028. **Table 12** summarizes the costs associated with these priority road works.

Table 12: Priority Road Project Considerations

Road Rehabilitation	Costs
Geotechnical Assessment of Priority Roads	\$25,000
Addition of priority roadside barrier	\$130,000
Priority Road Shouldering	\$9,000
Spot Repairs	\$30,000
Crack Sealing and Minor Patching	\$50,000
Mill & Overlay	\$947,000
Reconstruction of Local and Collector Roads	\$1,084,000
Total	\$2,275,000

It is important to note that costs are provided in 2017 Canadian Dollars and construction works include 35% for survey, Geotechnical and Civil Engineering, tendering, contract administration and contingency. Costs should be adjusted for annual budgeting to reflect inflation.

At the time of this report, the Village provided a breakdown of how the capital and operating budget is currently allocated for roads and infrastructure. The current budget allocation is summarized in Table 13 below:

Table 13: Current Budget Allocation

Budget Category Description	Allocation
Annual Capital Budget – MRN (only for MRN)	\$30,000
Annual Operating Budget – MRN (only for MRN) - includes snow removal budget of \$4,500 and vegetation budget of \$3,000; does NOT include portion of MRN funding that is used to cover S&B of Belcarra staff for work on MRN roads)	\$33,300
Annual Capital Budget, non MRN	\$124,000
Annual Operating Budget, non MRN roads (includes snow removal (\$7,000) and vegetation budget (\$7,000))	\$23,200
Trails/Tennis Court	\$7,000
Community Works Fund. Can be used towards most capital works (i.e. not just for roads but could be for water system or recycling/garbage system assets).	\$53,000
Current Budget for Capital and Operations - Roads and Infrastructure	\$270,500



Based on a \$270,500 annual budget, the Village could make significant progress over the next 10 years and address most of the identified priority roadworks. This would require the Village to allocate the bulk of the budget to road works which is unlikely as compromises will still need to be made to optimize the roads program and align with other priority projects. This further emphasizes the need to stretch the useful life of the roads with crack sealing and patching as well as have a geotechnical engineer determine if less expensive renewal options can be used.

Map 3 – Recommended Road Surface Works 2018 - 2028 provides the recommended rehabilitation method over the next 10 years for each road based on the visual inspection.

For reference, we have provided Class C cost estimates for the roads identified to be addressed in the next five years. These cost estimates can be found in Appendix B.



Summary of Recommendations

This RCA data, photos and videos taken during the field work will provide critical information to inform the Village's roads capital program. This information will also be valuable when planning other infrastructure projects. Combining the transportation and other priority infrastructure works will ensure the Village makes best use of their capital funds.

The Village of Belcarra should consider the following short-term recommendations that will provide immediate benefits while also helping to enhance the Village's ongoing road maintenance program:

1. Review and become familiar with the data collected from the RCA. There was a lot of information collected that can be used to help inform the Village's capital program including establishing level of service and investment levels to sustain the Village's roads. Information can be used to inform staff and Council decisions.
2. Address all health and safety concerns as soon as possible. If the pavement edge failures and shoulder drops are addressed, it will significantly eliminate the majority of the health and safety concerns;
3. Install guard rails at identified locations to reduce concern along Bedwell Bay Road and other key locations;
4. Undertake the shoulder repair and maintenance to help strengthen road edge in identified locations;
5. Undertake a crack sealing and minor patching program. That work will be critical to slow the deterioration of the Village's roads and may defer higher cost work;
6. Have a geotechnical review done for the roads to be rehabilitated over the next five years;
7. Use the RCA spreadsheet to assist in setting maintenance, rehabilitation and reconstruction priorities in conjunction with the priorities determined from the stormwater planning;
8. Continue to monitor, track progress and update the RCA each year, with a focus on roads that are experiencing changes in conditions; and
9. Re-establish curbs at identified locations that were affected by vehicle damage, tight corners and those affected by new construction.



COUNCIL REPORT

Date: February 21, 2023

From: Paula Richardson, Chief Administrative Officer

Subject: NAV CANADA - Changes to Flight Paths in Greater Vancouver Region and Southern Vancouver Island

Recommendation:

That staff be directed to write a letter to NAV CANADA expressing concern with proposed route changes to flight paths over the Tri-Cities area for aircraft accessing Vancouver International Airport.

Purpose:

The purpose of this report is to bring to Council's attention the proposed changes to flight paths in the Greater Vancouver Area particularly in the Tri-Cities area.

Background:

The core business of NAV CANADA is to provide air navigation services, primarily within Canada. NAV CANADA is responsible for helping aircraft safely navigate the 18 million square kilometres of Canadian airspace and the North Atlantic oceanic airspace under Canada's control.

In 2022, NAV CANADA put forward proposed changes to route changes and approach procedures at Vancouver International Airport which will particularly impact Coquitlam, Port Coquitlam and Port Moody. The organization held public consultations from December 6, 2022 to February 1, 2023.

The Cities of Coquitlam, Port Coquitlam and Port Moody have all expressed concerns with the changes and the impact on their communities.

The Village of Belcarra was not involved in public consultations, however, staff feel it important to support our Tri-City neighbours as well as our own residents and forward a letter to NAV CANADA listing our concerns with the proposed changes to flight paths.

Appendix A: NAV CANADA publication: Changes to Flight Paths in Greater Vancouver Region and Southern Vancouver Island

A wide-angle photograph of Vancouver International Airport. In the foreground, the airport's terminal and various ground service vehicles are visible along the waterfront. In the background, a large commercial airplane is in flight against a clear sky. The airport is situated at the base of a range of snow-capped mountains.

Changes to flight paths in Greater Vancouver Region and Southern Vancouver Island

Changes in communities to the north and northeast of the airport

INTRODUCTION

The objective of the Vancouver Airspace Modernization Project (VAMP) is to enhance safety, modernize procedures, and ensure the airspace structure can accommodate the demand for air services. The project proposes changes to the instrument approach procedures for Vancouver International Airport (YVR) affecting a broad area around Metro Vancouver, with some places more affected than others.

While the focus of the Project was mainly on designing and introducing new instrument approach procedures for Vancouver International Airport, some procedures will remain the same – such as departure procedures and procedures used by aircraft operating under Visual Flight Rules (VFR) (such as helicopters or floatplanes).

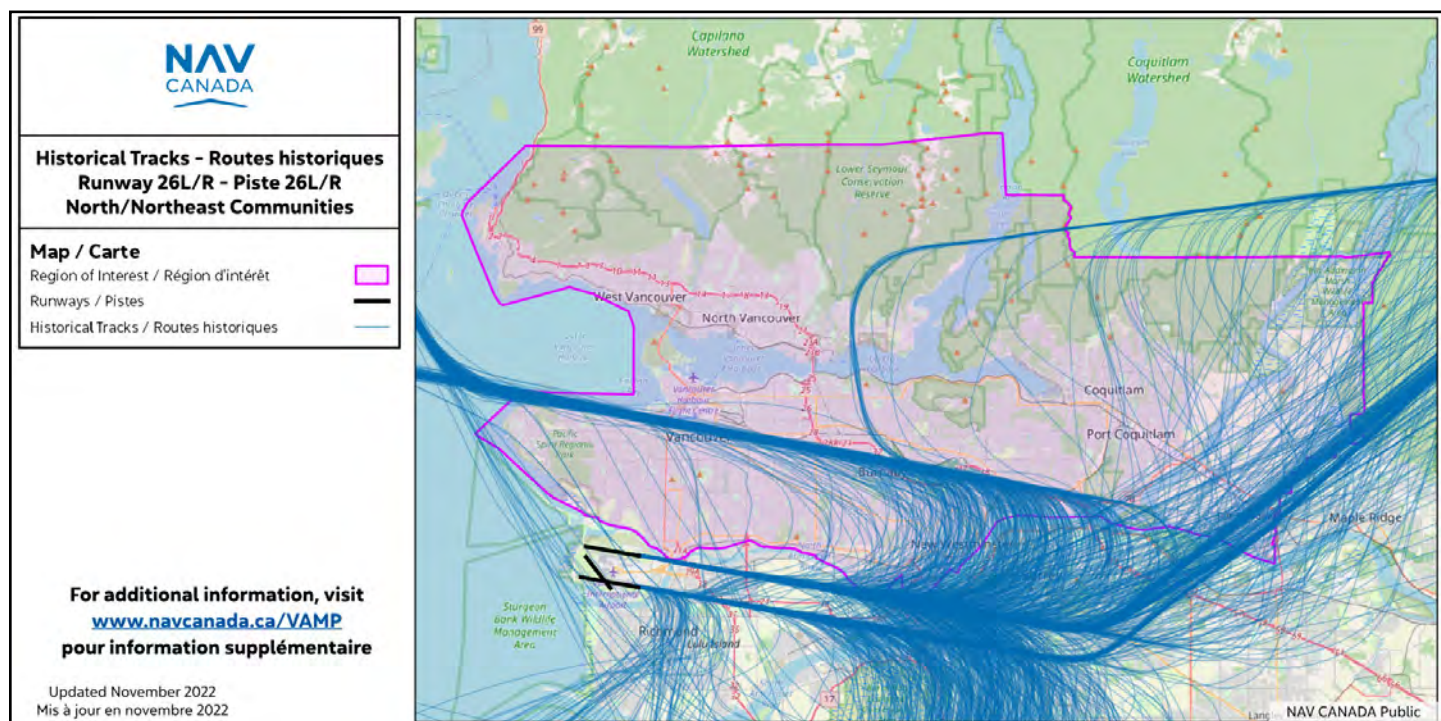
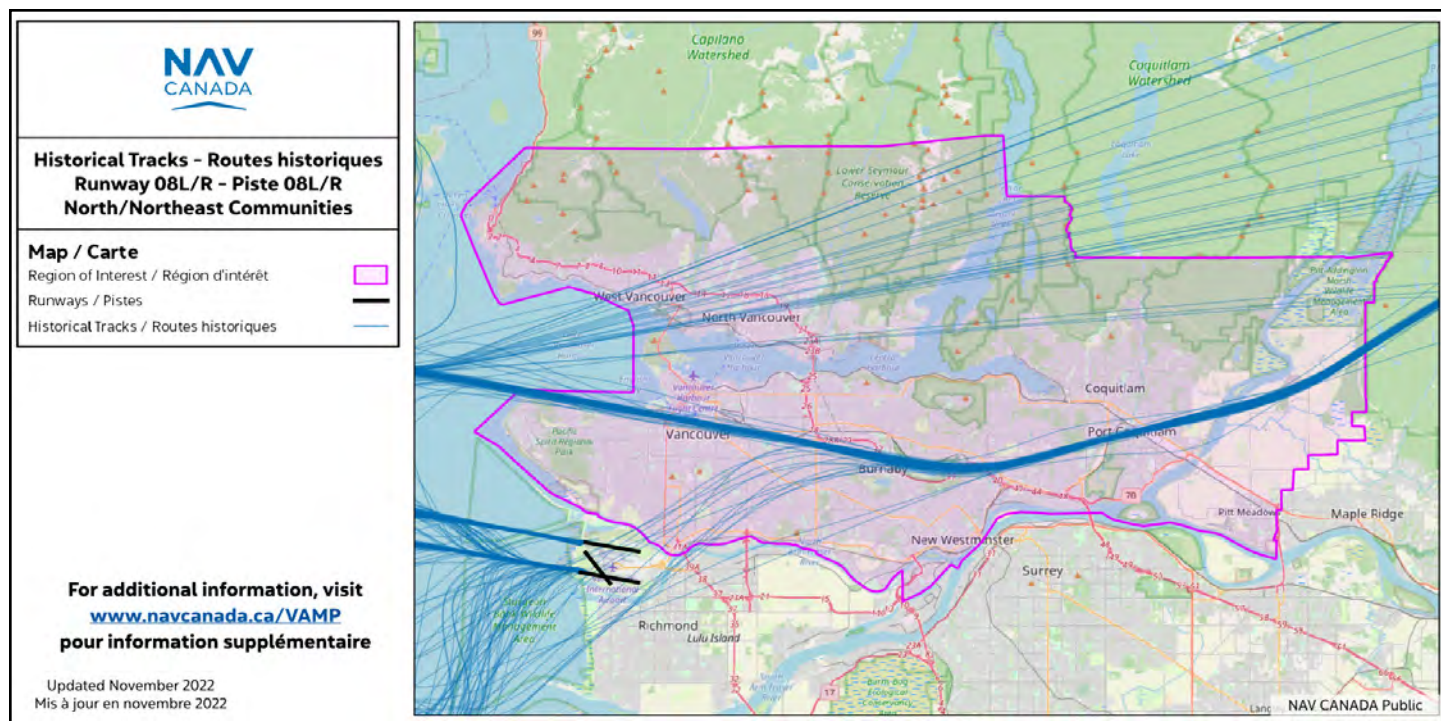
The proposed RNP AR procedures will allow aircraft to line up with the runway sooner than when using a typical procedure today. As a result, aircraft will fly a shorter distance and consume less fuel, and will also be operating on a Continuous Descent profile, which enables an aircraft to descend on a quieter reduced engine setting.

This document provides information on current procedures and flight paths as well as proposed changes affecting communities located north and northeast of YVR.

CURRENT OPERATIONS

For context, the images below show samples of arriving traffic over a few busy days in 2019 with existing procedures in place. The active runway is determined by wind conditions at the airport – for safety reasons aircraft must land and take-off into the wind. The first image shows aircraft arriving eastbound using runways 08L and 08R when winds are blowing from the east. The second image shows aircraft arriving westbound using runways 26L and 26R when winds are blowing from the west.

COMMUNITY-SPECIFIC MAPS:



As can be seen, aircraft do not all follow the exact same path when they are arriving and some are often directed (or “vectored”) by air traffic control to operate off the procedures. This is done to ensure safe sequencing or provide for more direct routing and this practice will continue in the future. Determining which end of the runway is used is based on many factors including wind direction and speed. How often each runway direction is used will not change directly as a result of the proposed approach procedures.

COMMUNITY-SPECIFIC MAPS:

PROPOSED CHANGES

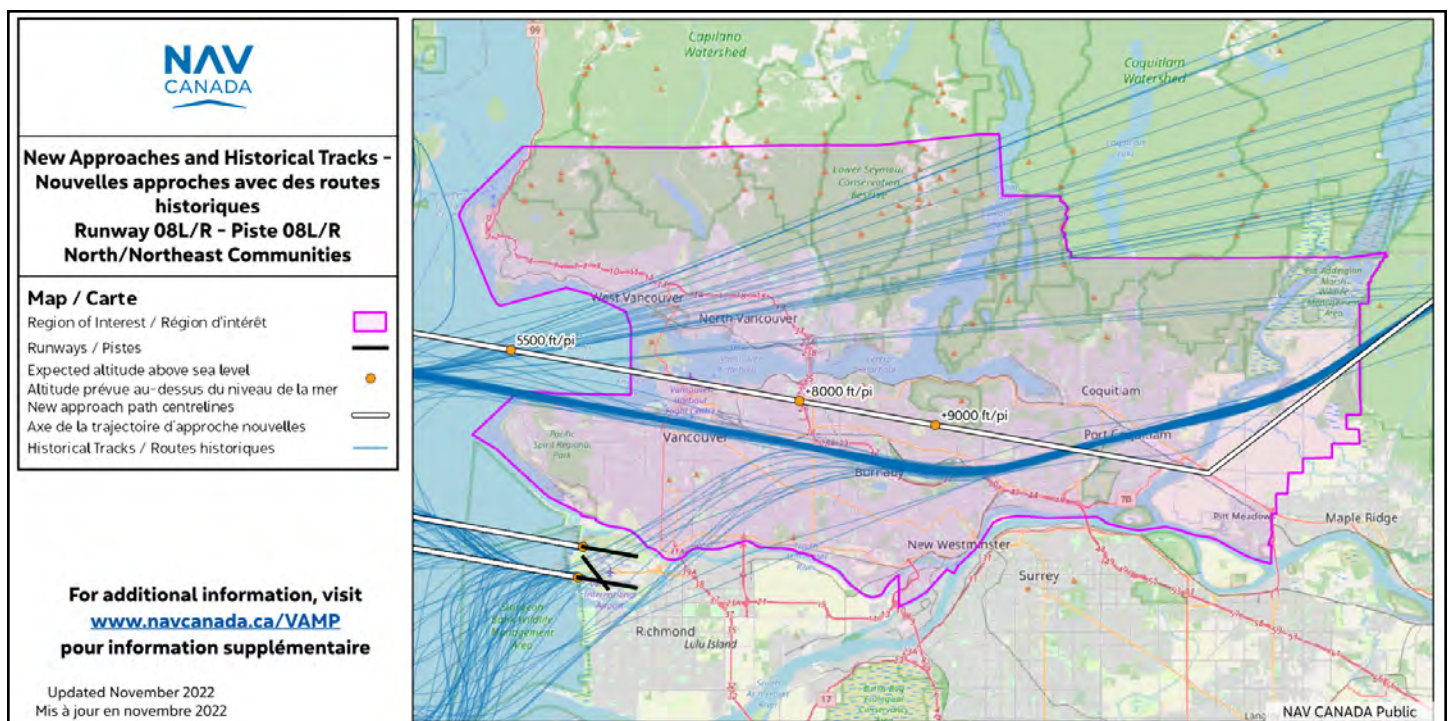
NAV CANADA is proposing changes to instrument approach procedures at YVR including changes to existing procedures, the addition of new satellite-based procedures, changes to some existing procedures, and changes to some of the arrival routes further away from the airport.

A key element of the project is the introduction of RNP AR approach procedures that defines a highly precise flight track including a curved segment along the approach which turns aircraft from the downwind segment (aircraft flying north of the airport in an east/west direction) to the final approach segment (when the aircraft lines up with the runway). This curved segment for the RNP AR procedure is different than what you see today as its path over the ground is precisely defined in advance and not subject to change on an aircraft-by-aircraft basis. Not every aircraft will use these curved segments.

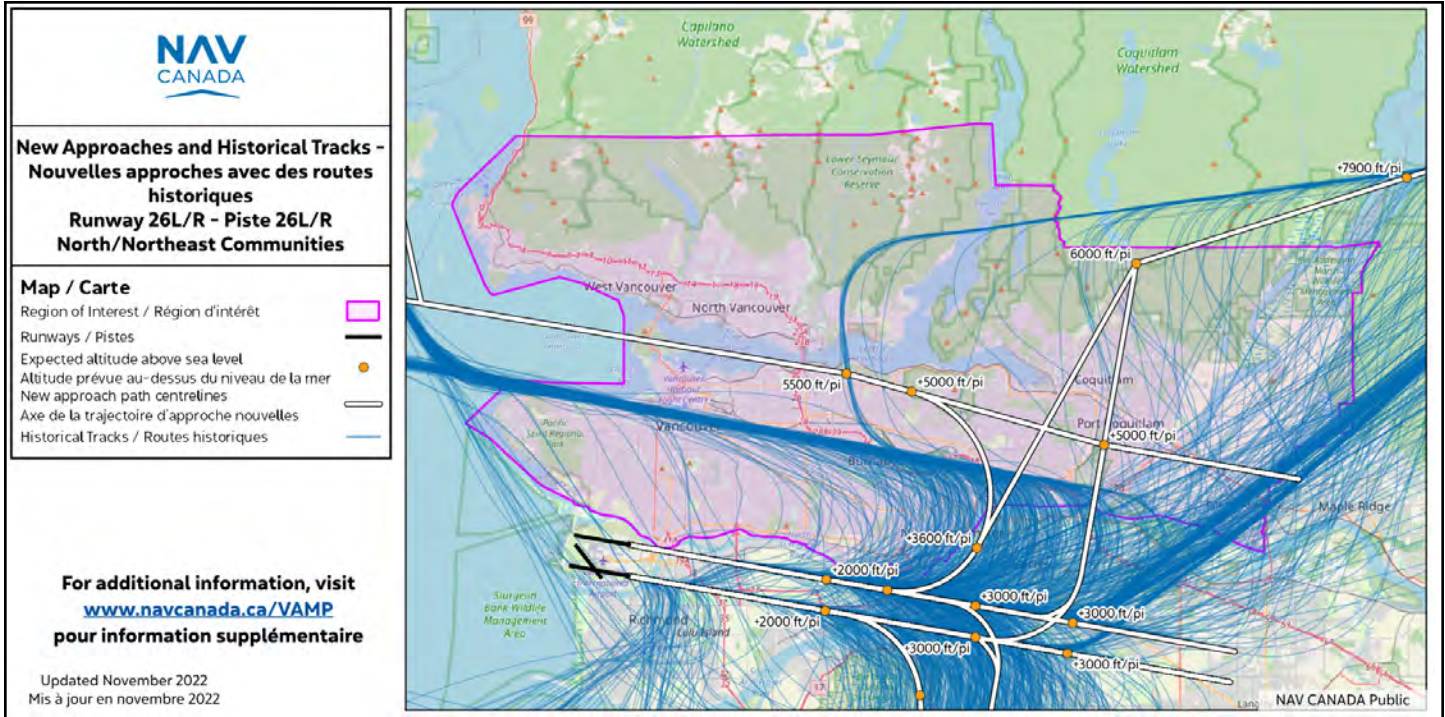
To support the introduction of these curved segments, the location of the northern downwind segment would move further north from its current location overtop the city to more over the water. This helps ensure aircraft have enough space to make their 180° turn and line up with the runway.

Aircraft in this area will utilize the new downwind location, but only a certain percentage are equipped with RNP AR technology and holding certification from Transport Canada will use the curved part of the approach. Aircraft not using the RNP AR approach procedure will use the downwind and will be instructed to initiate their base turn one-by-one by air traffic control, leading to a high variability for where these aircraft will overfly the area. It is also not expected that 100% of RNP AR-equipped aircraft will fly the curved approach due to operational considerations such as traffic levels, aircraft sequencing, or weather.

The images below show the location of the new arrival paths including the downwind segment and the curved path from downwind to the final approach along with the historical aircraft tracks shown in the earlier image. Another way to think about it is the aircraft shown in blue using the old routes would in the future fly the new routes and new downwind and some of those aircraft would take the shorter curved path.



COMMUNITY-SPECIFIC MAPS:



WHAT IT MEANS FOR COMMUNITIES

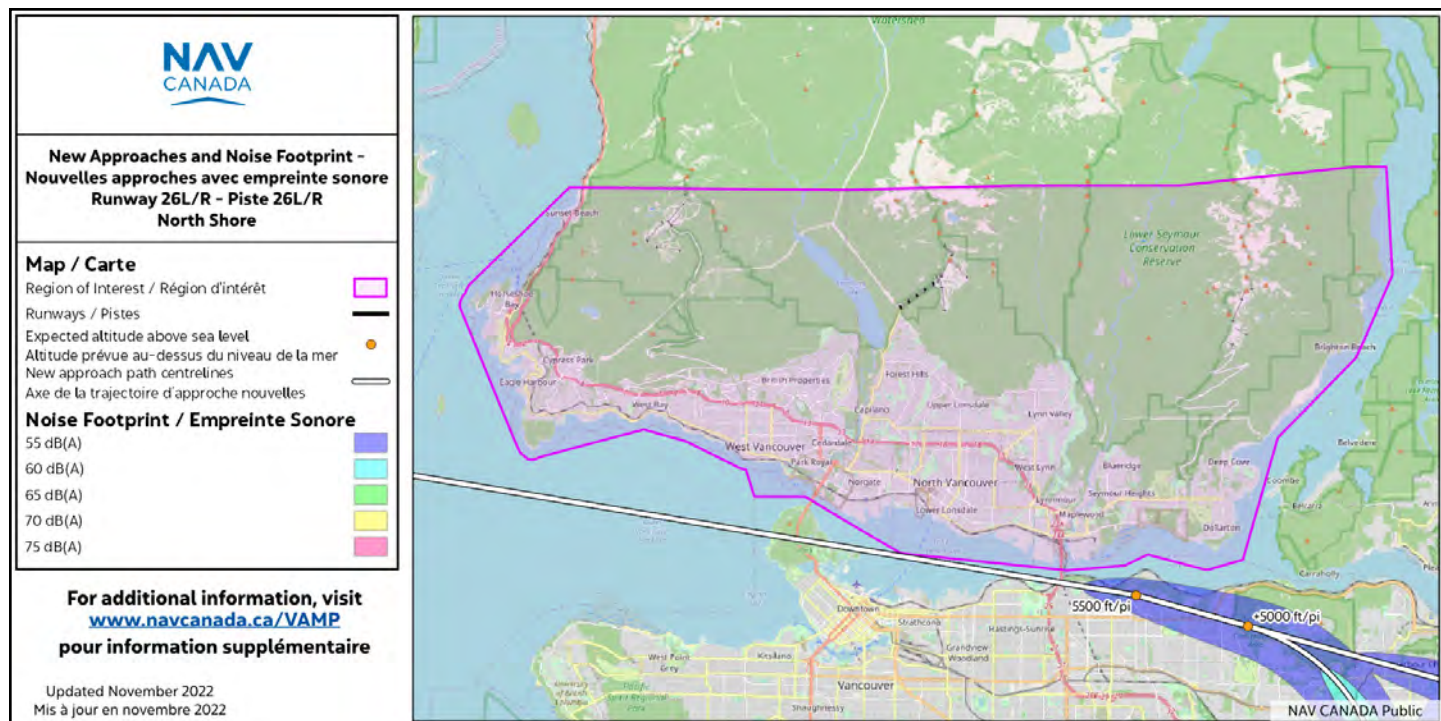
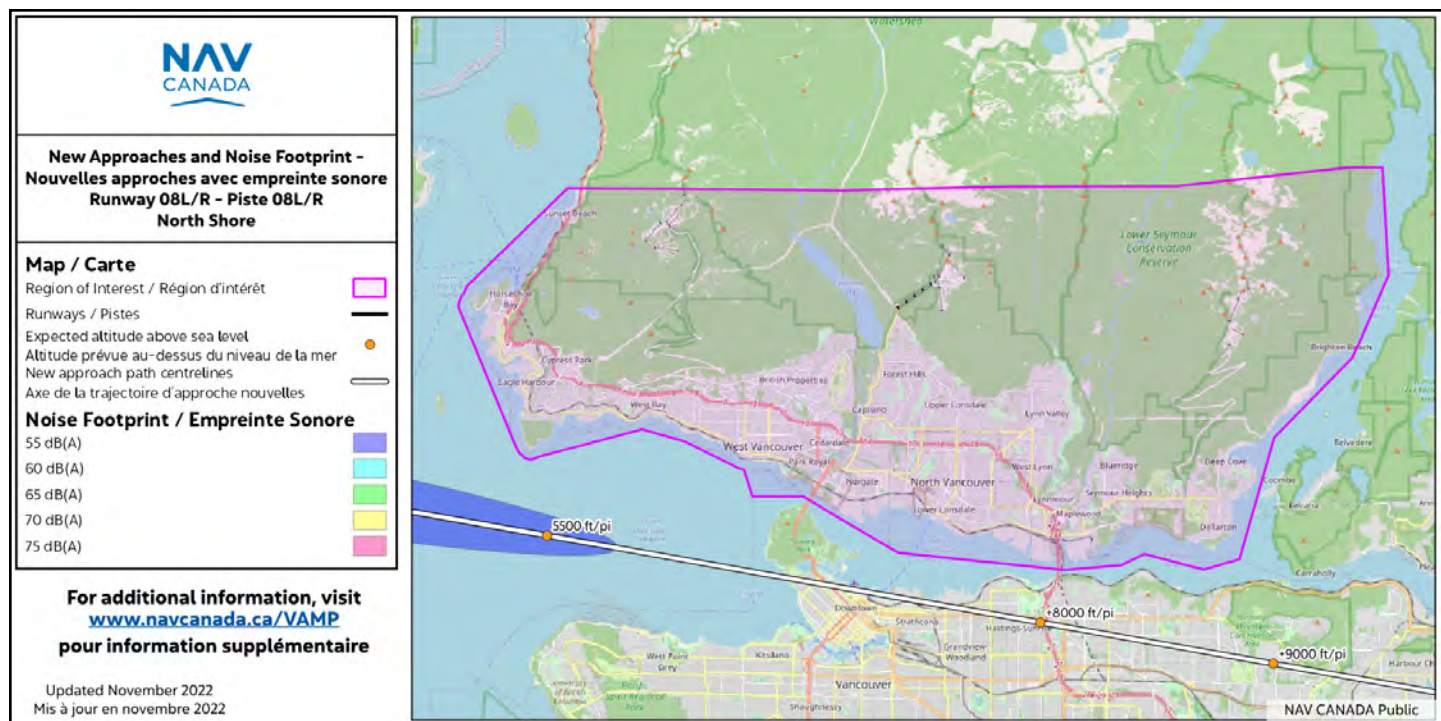
NAV CANADA assembled detailed information on aircraft operations for arrivals to YVR. This included specific aircraft fleet mix information such as aircraft type, arrival and departure times, and routes of flight. Using this data, noise modeling was conducted to better understand the noise footprint associated with the proposed procedures. Departures from YVR are excluded from noise modeling as there are no proposed changes to departure procedures.

When operating outside certain categories of controlled airspace, aircraft operating under Visual Flight Rules (VFR) are not always required to be in contact with air traffic control. Because these aircraft operate at the pilot's discretion along non-defined and highly variable routes, they have also been excluded from noise modeling.

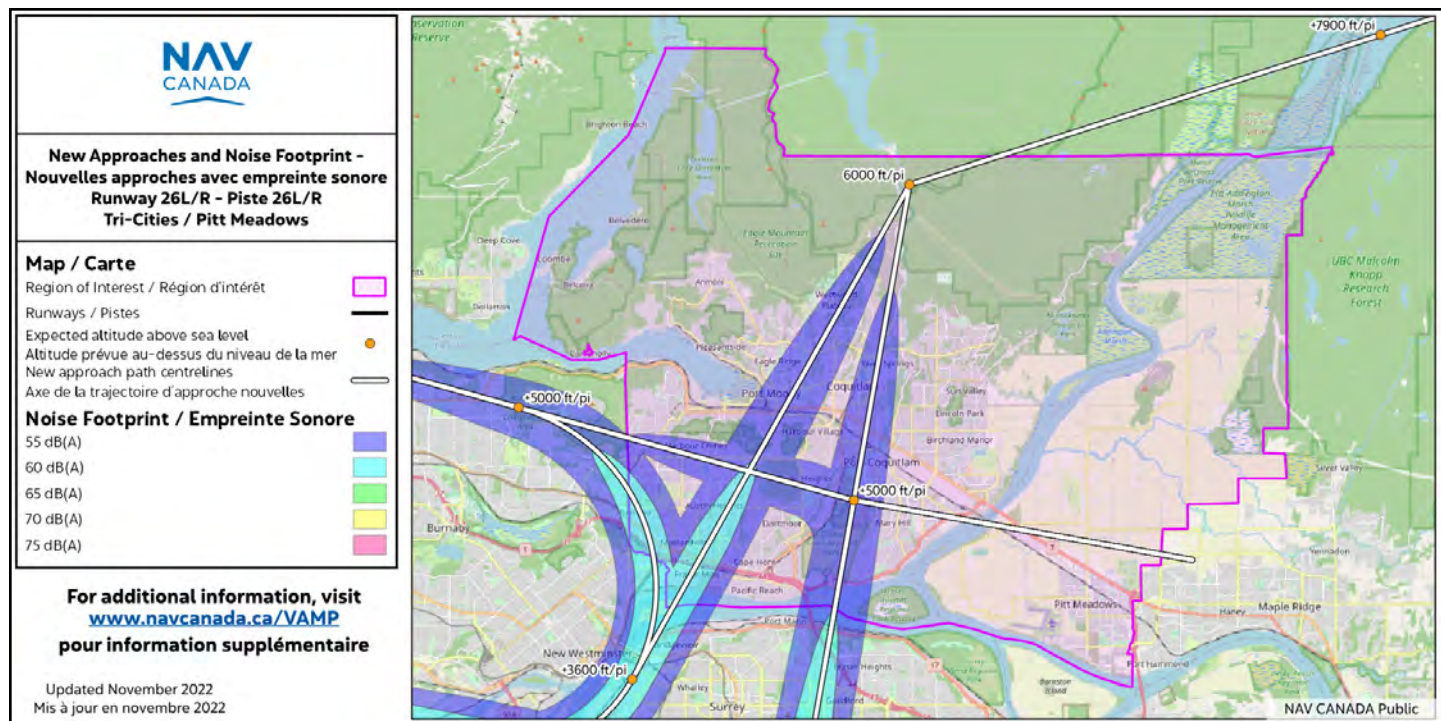
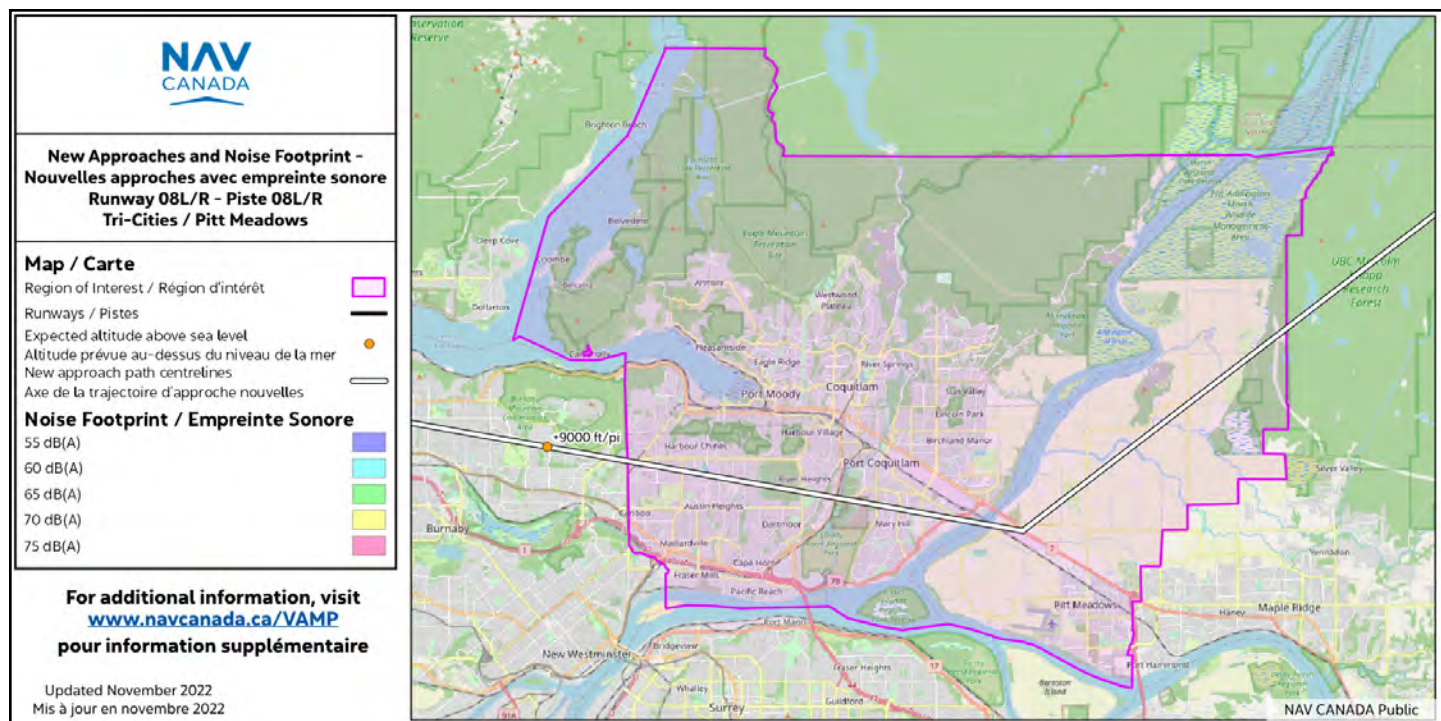
The design of instrument approach procedures must meet stringent national and international standards to meet a high level of safety and all efforts were made to mitigate noise whenever it was safe and technically feasible. Despite incorporating a number of noise mitigation measures into the proposal, it is important to note that entirely avoiding overflight of residentially populated areas is simply not possible and that some residents may observe aircraft operating more regularly in certain areas than they had before. Most areas surrounding the airport will continue to observe many of the aircraft operations that they do today, whether they are associated with arrivals or departures.

The images below show the location of the new downwind segment and the curved path from downwind to the final approach along with the noise "footprint" of a Boeing 737-800—a commonly used aircraft at YVR—conducting the arrival and approach procedure. Maximum sound level—expressed in decibels as dB(A)—is shown at various intensities using colours. Single-event noise level metrics represent the maximum noise level at a receptor location, considering a particular set of aircraft operations.

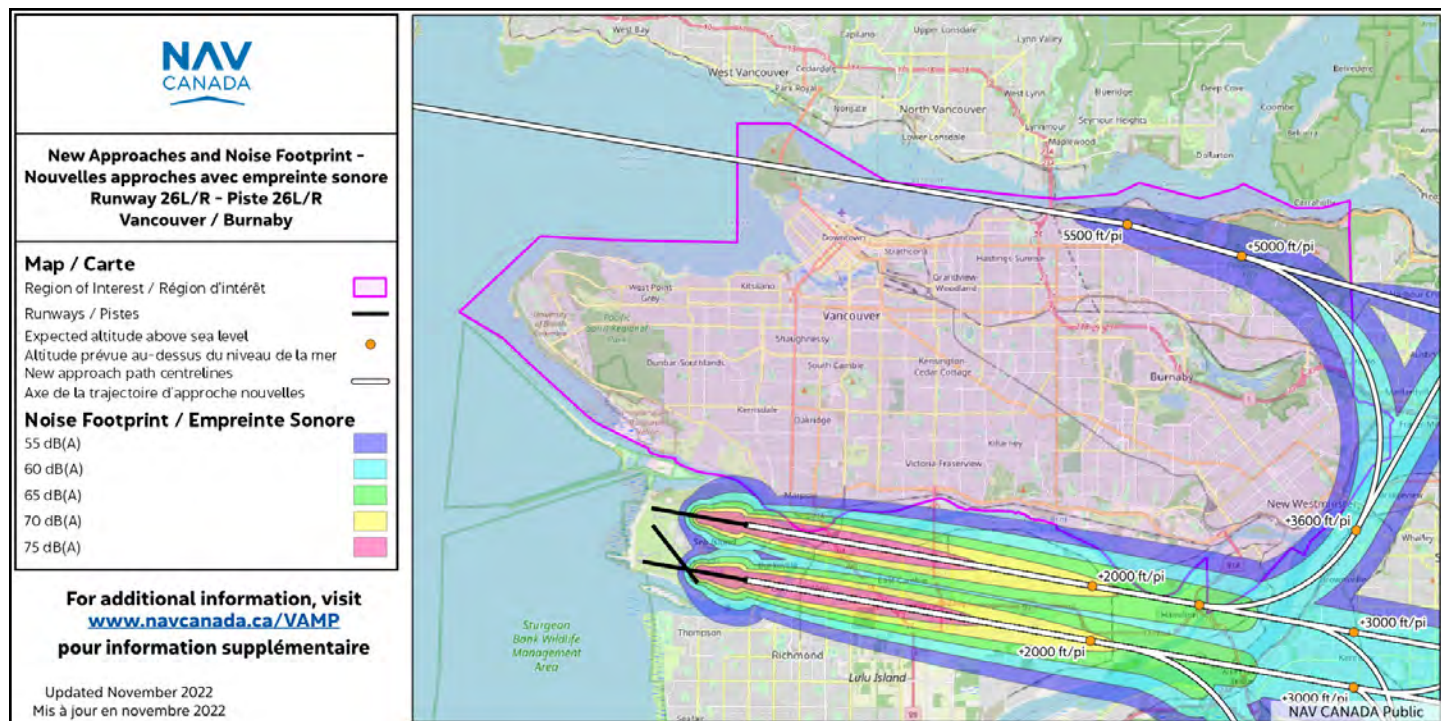
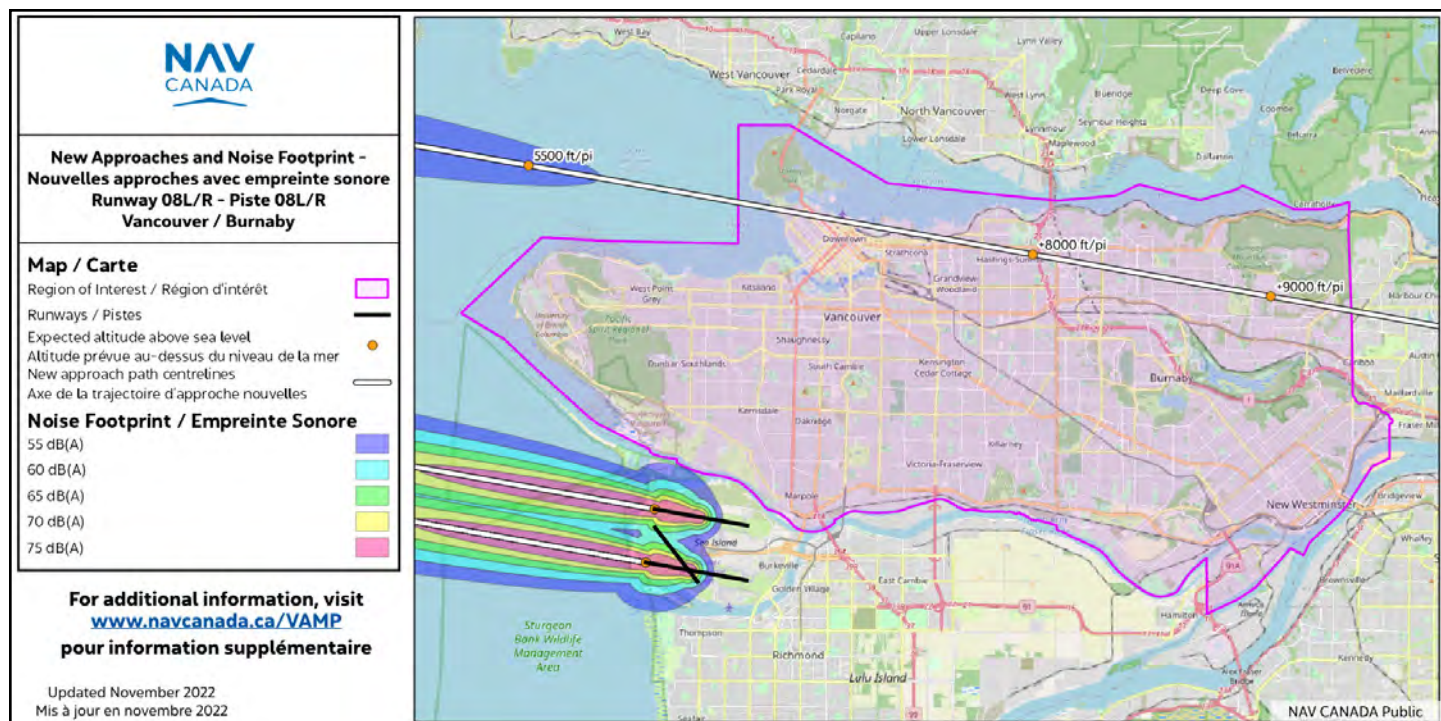
COMMUNITY-SPECIFIC MAPS:



COMMUNITY-SPECIFIC MAPS:



COMMUNITY-SPECIFIC MAPS:





**VILLAGE OF BELCARRA
Council Indemnity
Bylaw No. 604, 2023**



A bylaw to provide for the payment of an indemnity to
Village of Belcarra Mayor and Councillors

WHEREAS the Municipal Council may, by bylaw, provide for the payment from annual general revenue, an indemnity to the Mayor and to each Councillor for the discharge of their duties of office;

NOW THEREFORE the Municipal Council of the Village of Belcarra in open meeting assembled enacts as follows:

1. This Bylaw may be cited for all purposes as the "Village of Belcarra Council Indemnity Bylaw No. 604, 2023".
2. The indemnity for the Mayor starting March 1, 2023 shall be the gross sum of \$2,014.23 monthly.
3. The indemnity for each Councillor starting March 1, 2023 shall be the gross sum of \$1,007.12 monthly.
4. The indemnities provided for in Section 2 and 3 above shall be paid by the Chief Administrative Officer, save and except for the provisions of Section 5 hereof.
5. In the event of any member of Council being absent from three consecutive regular Council meetings, the indemnity that would otherwise be due to that member shall not be paid to that member. This provision may be waived by a unanimous vote in favour thereof by the remaining members of Council.
6. If a portion of this bylaw is held invalid by a Court of competent jurisdiction, then the invalid portion must be severed, and the remainder of this bylaw is deemed to have been adopted without the severed section, subsection, paragraph, subparagraph, clause or phrase.
7. This bylaw shall take force and come into effect as of March 1, 2023.

8. The "Village of Belcarra Council Indemnity Bylaw No. 595, 2021" is hereby repealed.

READ A FIRST TIME on January 23, 2023

READ A SECOND TIME on January 23, 2023

READ A THIRD TIME on February 6, 2023

ADOPTED by the Council on

Jamie Ross
Mayor

Paula Richardson
Chief Administrative Officer

This is a certified a true copy of
Village of Belcarra Council Indemnity Bylaw No. 604, 2023

Chief Administrative Officer



PMSS Parent AfterGrad

9.1

300 Albert St., Port Moody, B.C. V3H 2M5

PMSS Parent AfterGrad 2023

Dear Donor:

The students and parents of the **Port Moody Secondary School Graduating Class of 2023** need your help.

In keeping with PMSS tradition, every year the students, parents and community join together to create a wonderful safe dry AfterGrad celebration. We are working to ensure that our students enjoy a safe fun-filled event, and that they leave PMSS with great memories of their high school Graduation Ceremony and After Grad Celebration; memories that will last a lifetime.

We understand that these are challenging times for businesses throughout our community, but your generous contribution can play a significant role in helping the AfterGrad Committee offer the best event possible for our students.

We are asking for a donation of cash that will help offset the cost of putting on the event, or goods or retail store services that will be given away as prizes during the AfterGrad event. Your contribution will make AfterGrad 2023 a night to remember. Many of you have contributed in previous years, and we hope that we can count on you again this year!

Companies, local businesses, or individuals will be *recognized* at PMSS through:

- PMSS Newsletter – The Times (distributed to over 800 students)
- Signage During the Dry AfterGrad Event

If you wish to make a donation, please contact Heather Skipworth (604-836-6570) – skippys@shaw.ca

Donations can be mailed to the address below or local pick up can be arranged.

Please make any cheques payable to “**Port Moody Secondary School PAC**” and put “AfterGrad” on the memo line.

Mail to: Heather Skipworth - AfterGrad Committee, Port Moody Secondary School, 300 Albert St., Port Moody, B.C. V3H 2M5

Thank you for your consideration and support.

The PMSS Parent AfterGrad Committee



February 6, 2023

Mayor Jamie Ross and Council
 Village of Belcarra
 4084 Bedwell Bay Road
 Belcarra, BC V3H 4P8
 via email: prichardson@belcarra.ca

Reference: AP7052

Re: 2021 Active Transportation Planning – Village of Belcarra Official Community Plan (OCP) Update

Dear Mayor Ross and Council,

Thank you for providing a final report and financial summary for the above-noted project. We have reviewed your submission and all reporting requirements have been met.

The final report notes a total actual eligible expenditure of \$20,000. Based on this, a payment in the amount of \$20,000 will follow shortly by electronic funds transfer. This transfer represents final payment of the grant and is based on 100% of the total reported expenditure (to a maximum of the approved grant of \$20,000).

I would like to congratulate the Village of Belcarra for undertaking this project and responding to the opportunity to incorporate or enhance active transportation components in formal planning documents.

If you have any questions, please contact Local Government Program Services at 250-356-7123 or lgps@ubcm.ca.

Sincerely,

A handwritten signature in black ink, appearing to read "L Pacey".

Lynsay Pacey, Program Officer
 Local Government Program Services

The Active Transportation Planning program is funded by the Province of BC

February 7, 2023

Dear Mayor and members of Council,

I am writing to bring to your attention the critical need for an emergency alert system serving the boat-access communities of Indian Arm and adjacent municipalities, including Belcarra. Due to the remote location of these communities, response from local residents can often be faster than that of other emergency services during the initial stages of an emergency. A prompt and reliable emergency alert system would support the engagement of local residents in initial emergency response, thereby buying time for the arrival of better equipped and trained responders such as the SVFD.

Recently, on February 3, 2023, a house in the Twin Island area experienced a chimney fire that posed a threat to the entire structure. During a high-wind event, the fire had the potential to spread quickly. However, due to the quick response of Indian Arm residents from multiple municipalities and verbal support from the SVFD, the risk of the fire spreading was reduced before the arrival of SAR2/SVFD personnel. Upon their arrival, they were able to extinguish the fire and confirm the safety of the building. This close call highlights the importance of a prompt and reliable emergency response system.

This is the third fire emergency in the past three years in the remote areas of Belcarra that has seen initial response by Indian Arm residents and boaters. The quick response from these individuals has helped prevent more significant structural fires and the spread of fire to the surrounding wilderness. Nevertheless, this initial response relies on informal phone calls and social media postings, which are unreliable and often fail to reach all members of the communities in a timely manner. This can result in a delay in response that can have severe consequences, despite there being many residents willing and able to respond if notified.

On September 6, 2022, Council considered the use of the Alertable app to provide an emergency alert system, but the purchase was deferred pending further exploration by the SVFD. I respectfully request that Council reconsider this initiative as a top priority and explore collaborative opportunities with adjacent municipalities, including the District of North Vancouver, Metro Vancouver Electoral Area A, and Port Moody. I understand that Metro Vancouver already has an emergency notification system for residents of Electoral Area A, but I am not aware of its use and trigger mechanisms.

Investing in a reliable and effective emergency alert system will ensure that residents of Indian Arm and Belcarra are better informed and prepared to respond promptly in times of emergency. The residents of these communities have always been proactive in their response to emergencies, but with timely notification and a coordination mechanism across municipalities, we could ensure the safety and security of our communities to an even greater extent.

Thank you for your time and consideration.

Sincerely,

David Barwin
Belcarra, BC



February 10, 2023

Ref: 272022

Dear Mayors and Regional Chairs:

I am pleased to let you know of the significant investment our government has made to support all our municipalities and regional districts around the province. This is in direct response to my mandate letter to support growing municipalities and regional districts with funding for infrastructure and community amenities.

Today Premier David Eby and I announced the [B.C. building stronger communities with \\$1-billion Growing Communities Fund | BC Gov News](#). The fund will provide a one-time total of \$1 billion in direct grants to all 188 of B.C.'s municipalities and regional districts. Your local government can use it to address your community's unique infrastructure and amenities demands, such as recreation facilities, parks and water treatment plants, as well as other community infrastructure. It will help communities prepare for future growth and build the amenities needed to support new home construction, especially with the *Housing Supply Act* where targets are set.

These grants will complement existing infrastructure funding programs for projects such as sewer, water and recreation facilities. The province will distribute them to B.C.'s 188 municipalities and regional districts by the end of March 2023. The Growing Communities Fund arises from the surplus shown in the Second Quarter Financial Report. The province is putting this year's surplus to work for people to support them now and for the long term.

The province has heard from local governments about the need for infrastructure and amenities to support their growth. Infrastructure funding programs are routinely significantly oversubscribed. For example, there were six times more requests for funding through the "Investing in Canada Infrastructure Program Community Culture and Recreation" stream than what was available. This one-time fund also supports priorities identified by the Union of British Columbia Municipalities (UBCM).

The Ministry will issue a direction letter to each local government in March 2023 including further details on this one-time direct grant. This will include information on the formula used to allocate the funds, the amount your local government will be receiving and the province's expectations for the use and reporting of the funds.

As this is a direct grant from the province to each municipality and regional district in B.C., your local government will not have to apply for the funds. Your council or board will be required to make decisions on the use of the funds in compliance with this second letter coming in March 2023. Projects that support neighboring First Nations communities are strongly encouraged.

.../2

I trust you will join me in acknowledging the importance and value that this fund will have to focus on building a secure, low emission, sustainable economy and a province where everyone can find a good home – whether you live in a rural area, a city, or in an Indigenous community. Together we can make life better for people in B.C., improve the services we all rely on, and ensure a sustainable province for future generations.

I look forward to connecting with you again soon in person or virtually as I continue to tour and meet with local elected officials. In the interim, any questions can be directed to myself at: Minister.MUNI@gov.bc.ca. Staff are available at: LGIF.Infra@gov.bc.ca.

Sincerely,

A handwritten signature in black ink, appearing to read 'Anne Kang', with a stylized, cursive script.

Anne Kang
Minister
Ministry of Municipal Affairs

pc: The Honourable David Eby, Premier
 The Honourable Katrine Conroy, Minister of Finance
 Chief Administrative Officers
 Okenge Yuma Morisho, Deputy Minister, Municipal Affairs
 Jen Ford, President UBCM
 Gary MacIsaac, Executive Director, UBCM

From Walter Mechler

Port Moody, 2023-02-13

To Mayor Ross and the Council of the Village of Belcarra

Freedom of Information &
Protection of Privacy Act
Section 22(1)

(Severed portions are shaded)

Subject: Requesting again that Council consider separating and not charging the cost of operation and maintenance (O & M) of the recycle / refuse utility fee for non-users whose primary residence is elsewhere in Metro Vancouver.

Dear Mayor Ross and Councillors,

Mayor Ross, you will remember my previous attempts in 2000 and 2009 to get exempted from paying the solid waste (SW) utility fee. We have been paying the full SW utility fee although nobody of our family ever brought anything to the Belcarra SW facility, even when staying at Farrer Cove for weeks at a time. One has to drive 'into town' every few days to shop for food, check for mail and do other business. Delivery on these trips to the SW facility in the places where our family members live is convenient and saves the time and CO2 for detouring to the Belcarra SW facility.

Consequently, we did not take out the 'card' to enter the Belcarra SW facility site.

I must briefly introduce myself to councillors not familiar with our situation and antecedents. I am one of the owners of the property [REDACTED]

[REDACTED] which is west contiguous with the land of Cape Howdy. I

acquired the property 50 years ago, when there was one residence nearby besides Camp Howdy, which was then owned by the YMCA. Ten years later,

I sold undivided quarters to our three sons. When one or them died, his quarter was inherited in equal parts by his two sons who are married and have children. So, the place is now used by 4 generations intermittently throughout the year.

I appeared at a council meetings during the said years with a similar request. In 2000, Jamie Ross, at that time a councillor, arranged for the then superintendent of public works, Larry Scott, to look into this. His opinion was not favourable to my request, among other reasons because the cost of O & M would then have to be distributed over a smaller user group. That neglected to consider that there are just a few non-users paying the full fee. At present, one will not be far off assuming that the O & M portion of the \$417 utility fee will be around $\frac{3}{4}$ of the total.

Redistributing, say, \$ 310 to all users would be about one Dollar per year for each. Likely, other non-users would also be subject to the Speculation and Vacancy Tax (SVT) and have their primary residence not in Belcarra.

They should not be hard to find in the village records. For several reasons, after 2009 I did not pursue the request for separation of O & M cost for the purpose of having it waved for non users. One of them was to avoid the aggravation at my advanced age (now 97 years) and that of my wife of 71 years, who died last December, 95 years old.

I changed my mind when, now, I found that over the years I have paid a total of \$5275 Dollars, not counting foregone interest, without receiving a

tangible benefit. The annual utility fee has risen to \$ 417 with a history of a substantial increase every year. The annual amounts of the several items that make up the annual O & M cost are available as budget items before and as actual expenditures after each year. They are the expenses for running the SW facility and the much larger costs for the contractor handling and processing (?) the wastes and recyclables, and delivering them to Metro Vancouver facilities, and finally the charges levied by Metro Vancouver.

Similarly, the Capital/Replacement costs are established for each year. They are the expenses for capital works and amounts paid into funds to accumulate reserves for contingencies and replacements, as well as minor amounts that also relate to the SW utility but are not O & M. The Capital/Replacement costs are to be paid for each property as before. Being located in Belcarra, our property at Farrer Cove is subject to the speculation and vacancy tax (SVT). In a memorandum, which I sent to recipients of our charitable donations, I explained that this tax requires us to curtail donations. I attach a copy because it contains information on how our property was financed and improvements built with our own effort. I myself spent much time during my retirement building the house and outbuildings, sometimes with the help of family members. Several different local contractors were employed.

As you will know, the owner can avoid paying the SV Tax by renting the

property out for half the year or an aggregate of that length of time. My memorandum further states our reasons for not choosing to rent out our property to avoid paying that unjust tax, which for us last year rose by \$1100 to the total of \$3750, just like that, because the assessed value increased by 24%. The modest increase this year raises the SVT to \$ 4000.

While I said in the memorandum of November 2019 that we would cease making donations, in fact, we continue longstanding support of a few, now with larger amounts (the Eagle Ridge Hospital Foundation and the German-Canadian Benevolent Society which intends to rebuild the care home in Vancouver where our mothers lived their last years).

My memorandum contains excerpts from a paper by two principals of a Vancouver firm of real estate lawyers which confirms our suspicion from reading the tenancy act that it is slanted in favour of tenants. Considering this and all the other reasons for avoiding to be a landlord, one would be foolish to get involved in this particular rental business, if one is able to avoid it. Circumstances may not allow this for other owners of properties subject the SVT.

It would be of interest to affected owners, and should also be of interest for the Council to learn what impact the SVT has had and till has on property owners subjected to it. Answers to the following questions and

similar information would be relevant:

- ⊖ How many properties in Belcarra are affected.
- ⊖ How many are paying the tax and what amount currently, because of inability to find a tenant or not wishing to become a landlord.
- ⊖ Did an owner have to sell the property because of inability to pay the tax.
- ⊖ Did an owner attempt to rent it out but was unable to find a tenant and thus had to pay the tax.
- ⊖ Did one or more avoid the tax by letting a tenant use all or part of the property. What was the experience.

There may be privacy issues preventing one to ask owners of properties subject to the SVT these questions. But one may perhaps canvas owners if they were interested to contribute and get this information as well as their postal address and the location and perhaps a brief description of the property subject to the SVT.

Probably, there are others in the same situation as we are, namely having primary residences in the Metro Vancouver area where they use the local solid waste utility and thus also are non-users of the Belcarra facility.

If Council agrees that my request has merits and agrees to act on it, the first task would be to find the names and contact information of any non-users who now pay the full fee.

If clarification or further information is desired, please contact me over Tel

[REDACTED] at Port Moody or [REDACTED] mobile, or by E-mail

[REDACTED]. My address is [REDACTED]

[REDACTED] I still drive, but would rather not drive to a meeting in Belcarra when it is dark. I could try to get a family member to drive me.

Walter Mechler
Walter Mechler

Freedom of Information &
Protection of Privacy Act
Section 22(1)
(Severed portions are shaded)

Why we have to curtail charitable donations

The shot explanation is this: The NDP government of BC is taking away from us thousands of Dollars each year because we will not, cannot, rent to strangers for half of each year the house we built as family recreational home 35 years ago in the remote forest area north of Sasamat Lake that is now part of Belcarra. The Village of Belcarra, alone of similar communities with a significant number of 'cottages' or recreational homes, is in the area where these were made subject to the Speculation and Vacancy Tax. If you care to learn why we do not want to become landlords, continue to read this page and the next two.

At this time of the year we, Walter and Inge Mechler of [REDACTED] in Port Moody, used to make our donations of between substantial and token amounts to about two dozen recipients. I am 94 years old and caregiver to my wife Inge (92 yrs) who had 3 falls this year, the first two causing fractures of the left femur and the last fall, on the 8th of October causing the right femur to break. The fractures were repaired at The Royal Columbia Hospital. Each time she stayed at Eagle Ridge Hospital for rehabilitation until she was allowed to return into my care at home.

I had intended in any case to write to all but a few recipients and request they delete us from their donor list and save the expense of solicitation reminder. This year something happened that makes us to suspend all donations. The NDP government with the support of the lone Green MLA made our family vacation home in North Belcarra subject to the Speculation and Vacancy Tax (SVT). The Act with that name was assented to on the 27th of November 2018, but made retroactive to the first of January 2018. It requires us to pay several thousand dollars per year unless we rent it out for 6 month of the year.

I bought the property on [REDACTED] of Indian Arm in 1972. In 1984, I started building a house with the help of local contractors and family, but mostly built it myself during my retirement from work as a civil engineer making use of my earlier training as a carpenter. In 1987, I made our three sons co-owners with one

undivided quarter of the property each. My wife contributed funds from her work as a draftsman for which she trained at VVI during 3 years of night school. In fact, all along we have paid the entire costs in connection with the [REDACTED] property in North Belcarra, designated [REDACTED]. We want to continue with this as long as we can as a legacy to the family.

With two married grandsons having inherited the share of their deceased father there are now 3 generations of the family as owners of the property and four generations, all living in the Metro Vancouver area, are enjoying it **intermittently throughout the year.**

It is unlikely that people would want to rent living accommodation for just the darker and colder half of the year in an out-of-the-way place 3 km from a highway that leads to the city with infrequent public transportation. The only safe way to travel the 3 km to the highway in the dark is by car. About half of it is a pot-holed gravel road through forest, and part of that is through private land.

From our point of view, renting our property is out of the question for the following reasons among many others:

- We all have stuff in the house that we would have to clear out, store somewhere and return it when the tenant is gone.
- The hassle of drawing up a tenancy agreement for a fixed term when one has no experience of being a landlord and does not want to be one.
- Implications for our property insurance which includes our apartment in Port Moody.
- The risk of damage to our property by the tenant due to ignorance, neglect, or malice.
- Recent changes to the Residential Tenancy Act (RTA) by the NDP government which allow the tenant to overstay the end of the term lease.

On the last point I quote hereafter a paragraph from an opinion published on March 18 of this year by K. Pazder and M. Valana of the Pazder Law Corporation, real estate lawyers in Vancouver, titled "The SVT—What the Government isn't saying". It is worth reading the whole paper for anyone interested in this Act "conceived in the ideological kitchen of the NDP", as a major national news magazine characterized it.

"Even if you can find someone who agrees to rent for six months, recent changes to the Residential Tenancy Act (RTA) have now made a term lease very difficult to terminate. The amendments to the RTA prohibit landlords from ending fixed term tenancy agreements when they expire except in very limited circumstances. Therefore, your tenant is under no legal obligation to vacate the premises at the end of the fixed term."

If the tenant does not vacate and there is no new agreement, the tenancy will continue on the original terms from month to month [RTA section 44 (3)].

As most owners of a recreational property, we have in no way contributed to the housing crisis and resent being lumped in with real speculators, often from abroad, who have contributed to driving up the cost of real estate and often just let it sit empty. Our second home is shared by several generations of the family throughout the year. Its partial expropriation or else payment of the unfair tax is regarded by us as a legalized money grab which is all the harder to take because the Village of Belcarra is subject to the SVT while 2nd homes in other similar communities, in Whistler, Bowen and even in Lions Bay (which is closer in travel time to downtown Vancouver than Belcarra), are exempted.

*We hope you understand that at our age and with our health problems we have for some time wanted to reduce the number of donations to just a few with larger amounts. We trust that if you have taken the time to read the foregoing you will also understand that we decided not to make any donations while the unjust Speculation and Vacancy Tax is in effect. The tax brings us to the odd situation that every time in half the year when someone of our family is using our own property it costs like staying in a posh resort. **This is our resort and we have already paid for it.***

Walter Mechler